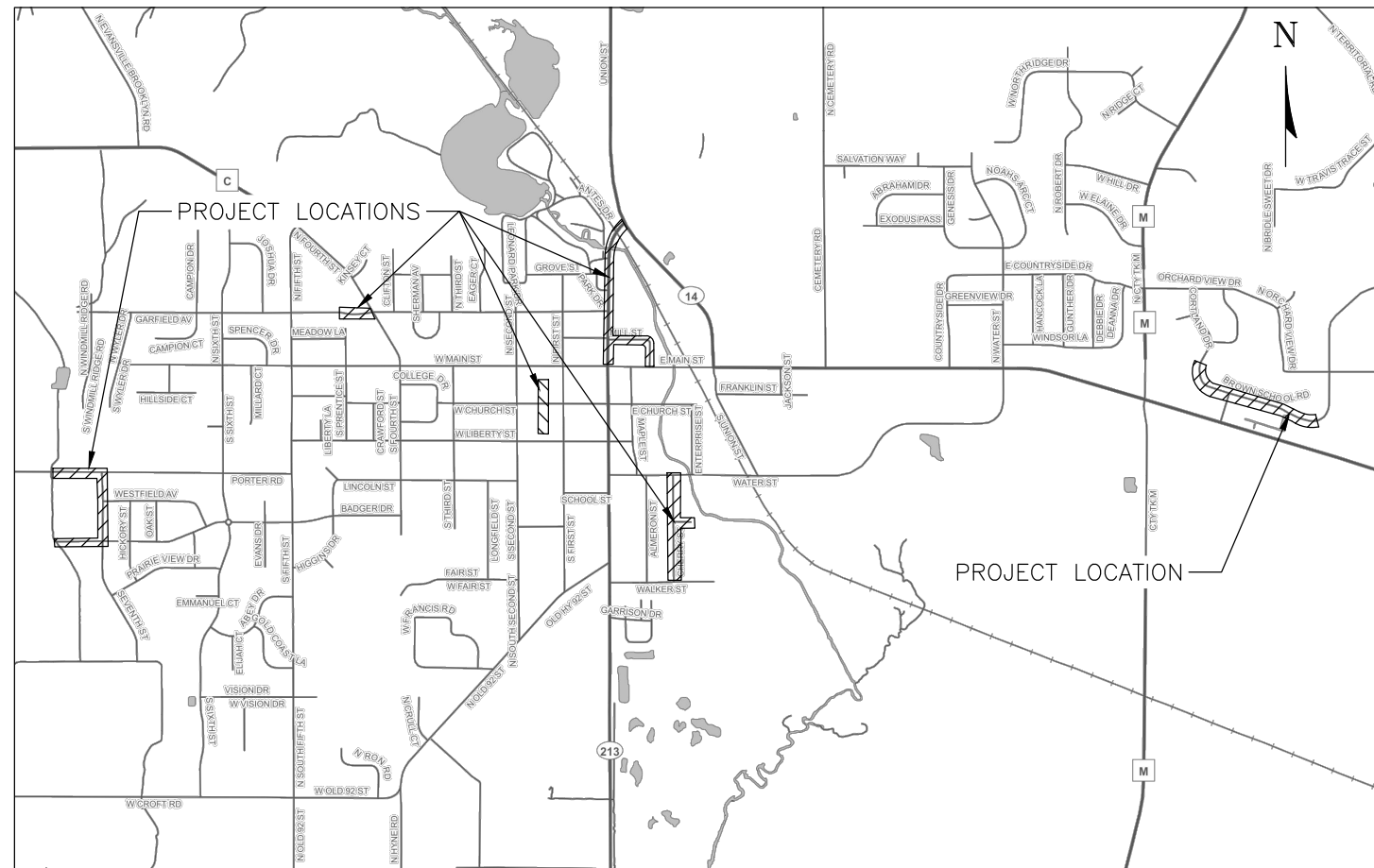


# 2025 STREET AND UTILITY IMPROVEMENTS

## CHERRY STREET, MILL STREET, AND RAILROAD STREET

City of Evansville, Wisconsin



NO SCALE



TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

CALL DIGGERS HOTLINE  
1-800-242-8511  
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WIS. STATUTE 182.0175 (1974)  
REQUIRES MIN. OF 3 WORK DAYS  
NOTICE BEFORE YOU EXCAVATE.

### LEGEND

UNDERGROUND TELE. — — — — — UT — — — — — UT — — — — — UT — — — — — UNDERGROUND CATV. — — — — — UCATV UNDERGROUND ELEC. — — — — — UE — — — — — UE — — — — — UE — — — — — UE — — — — — UE OVERHEAD OH — — — — — OH — — — — — OH — — — — — OH EXISTING GAS G — — — — — G — — — — — G — — — — — G — — — — — G PROPERTY LINE — — — — — EXISTING WATER MAIN — — — — — WM — — — — — WM — — — — — WM — — — — — WM EXISTING SANITARY SEWER — — — — — SAN — — — — — SAN — — — — — SAN — — — — — SAN EXISTING STORM SEWER — — — — — STM — — — — — STM — — — — — STM — — — — — STM EXISTING FENCE LINE — — — — — X — — — — — X — — — — — X — — — — — X — — — — — X SAWCUT XXX NEW STORM SEWER NEW WATER MAIN NEW SANITARY SEWER	<table style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td style="font-size: x-small;">WATER VALVE</td> <td style="font-size: x-small;">CURB STOP</td> <td style="font-size: x-small;">HYDRANT</td> <td style="font-size: x-small;">MANHOLE</td> <td style="font-size: x-small;">CURB INLET</td> <td style="font-size: x-small;">ENDWALL</td> <td style="font-size: x-small;">GAS WARNING</td> </tr> </table> <p>EXISTING ITEMS:</p> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td style="font-size: x-small;">FLAG POLE</td> <td style="font-size: x-small;">MAILBOX</td> <td style="font-size: x-small;">POWER POLE</td> <td style="font-size: x-small;">LIGHT POLE</td> <td style="font-size: x-small;">LAMP POST</td> <td style="font-size: x-small;">PULL BOX</td> </tr> </table> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td style="font-size: x-small;">WATER VALVE</td> <td style="font-size: x-small;">CURB STOP</td> <td style="font-size: x-small;">HYDRANT</td> <td style="font-size: x-small;">WELL</td> <td style="font-size: x-small;">MONITORING WELL</td> <td style="font-size: x-small;">TRACER WIRE</td> </tr> </table> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td style="font-size: x-small;">SANITARY MANHOLE</td> <td style="font-size: x-small;">SANITARY VALVE</td> <td style="font-size: x-small;">CLEANOUT</td> <td style="font-size: x-small;">STORM MANHOLE</td> <td style="font-size: x-small;">CURB INLET</td> <td style="font-size: x-small;">CIRCULAR INLET</td> </tr> </table> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td style="font-size: x-small;">SQUARE INLET</td> <td style="font-size: x-small;">ENDWALL</td> <td style="font-size: x-small;">STUMP</td> <td style="font-size: x-small;">DECID. TREE (RELATIVE SIZE SHOWN)</td> <td style="font-size: x-small;">EVERGREEN (RELATIVE SIZE SHOWN)</td> <td style="font-size: x-small;">SHRUB OR HEDGE</td> <td style="font-size: x-small;">SIGN</td> </tr> </table> <table style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> <td style="text-align: center;"></td> </tr> <tr> <td style="font-size: x-small;">CATV. PED.</td> <td style="font-size: x-small;">TELE. PED.</td> <td style="font-size: x-small;">ELEC. PED.</td> <td style="font-size: x-small;">GAS VALVE</td> <td style="font-size: x-small;">STREET SIGN</td> <td style="font-size: x-small;">IRON PIPE</td> <td style="font-size: x-small;">IRON ROD</td> </tr> </table> <p>NOTES: 1.) EXISTING FEATURES AND LABELS ARE SHOWN WITH SCREENED, LIGHTER LINES.                  2.) NEW CONCRETE IS SHOWN SHADED IN PLAN VIEWS                  3.) CONCRETE REMOVALS ARE SHOWN BY CROSS-HATCHING</p>								WATER VALVE	CURB STOP	HYDRANT	MANHOLE	CURB INLET	ENDWALL	GAS WARNING							FLAG POLE	MAILBOX	POWER POLE	LIGHT POLE	LAMP POST	PULL BOX							WATER VALVE	CURB STOP	HYDRANT	WELL	MONITORING WELL	TRACER WIRE							SANITARY MANHOLE	SANITARY VALVE	CLEANOUT	STORM MANHOLE	CURB INLET	CIRCULAR INLET								SQUARE INLET	ENDWALL	STUMP	DECID. TREE (RELATIVE SIZE SHOWN)	EVERGREEN (RELATIVE SIZE SHOWN)	SHRUB OR HEDGE	SIGN								CATV. PED.	TELE. PED.	ELEC. PED.	GAS VALVE	STREET SIGN	IRON PIPE	IRON ROD
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6264 Nesbitt Road  
Madison, WI 53719  
TOWN & COUNTRY  
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PROJECT NO.:	EV 124	DATE:	2-14-25
DRAWN BY:	A.M.C.	CHECKED BY:	N.R.B.
DRAWING FILE:	EV 124 DETAILS.DWG	REV. DATE:	

2024 STREET AND UTILITY IMPROVEMENTS
REVISIONS

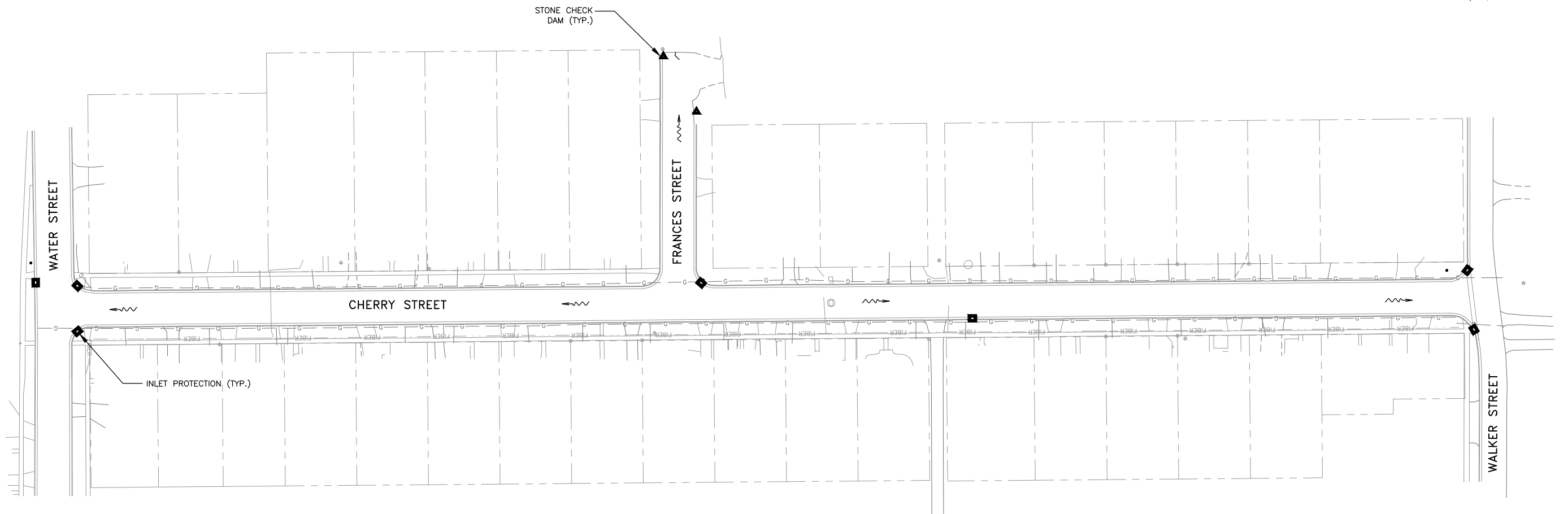
Cherry Street, Mill Street, and Railroad Street
SHEET

City of Evansville, Wisconsin
DATE

## SHEET INDEX

SHEET NO.	SHEET DESCRIPTION
1	INDEX
2	EROSION CONTROL PLAN CHERRY STREET AND GENERAL NOTES
3	EROSION CONTROL PLAN MILL STREET AND RAILROAD STREET
4	EROSION CONTROL – STANDARD CONSTRUCTION DETAILS
5	SANITARY SEWER – STANDARD CONSTRUCTION DETAILS
6	WATER MAIN – STANDARD CONSTRUCTION DETAILS
7	STORM SEWER – STANDARD CONSTRUCTION DETAILS
8	STREET IMPROVEMENTS – STANDARD CONSTRUCTION DETAILS
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A2	PLAN & PROFILE – CHERRY STREET STATION 105+60 TO STATION 111+60
A3	PLAN & PROFILE – CHERRY STREET STATION 111+20 TO STATION 117+20
A4	PLAN & PROFILE – FRANCES STREET STATION 10+00 TO STATION 16+00
A5	PLAN & PROFILE – MILL STREET STATION 200+00 TO STATION 206+00
A6	PLAN & PROFILE – RAILROAD STREET STATION 204+60 TO STATION 210+60
CURB & GUTTER AND STREET CONSTRUCTION	
B1	PLAN & PROFILE – CHERRY STREET STATION 100+00 TO STATION 106+00
B2	PLAN & PROFILE – CHERRY STREET STATION 105+60 TO STATION 111+60
B3	PLAN & PROFILE – CHERRY STREET STATION 111+20 TO STATION 117+20
B5	PLAN & PROFILE – MILL STREET STATION 200+00 TO STATION 206+00
B6	PLAN & PROFILE – RAILROAD STREET STATION 204+60 TO STATION 210+60
B7	PLAN & PROFILE – RAILROAD ALLEY STATION 300+00 TO STATION 306+00
B8	PLAN & PROFILE – NORTH MADISON STREET STATION 400+00 TO STATION 418+00
B9	PLAN & PROFILE – BROWN SCHOOL ROAD
B10	PLAN & PROFILE – GARFIELD AVENUE STATION 599+60 TO STATION 605+60
B11	PLAN & PROFILE – CHURCH STREET PARKING NORTH
B12	PLAN & PROFILE – CHURCH STREET PARKING SOUTH

CROSS SECTIONS	
X1	CROSS SECTIONS – CHERRY STREET STATION 101+58 TO STATION 103+06
X2	CROSS SECTIONS – CHERRY STREET STATION 103+22 TO STATION 104+72
X3	CROSS SECTIONS – CHERRY STREET STATION 105+02 TO STATION 106+50
X4	CROSS SECTIONS – CHERRY STREET STATION 106+80 TO STATION 108+20
X5	CROSS SECTIONS – CHERRY STREET STATION 108+25 TO STATION 110+00
X6	CROSS SECTIONS – CHERRY STREET STATION 110+25 TO STATION 111+50
X7	CROSS SECTIONS – CHERRY STREET STATION 111+75 TO STATION 112+75
X8	CROSS SECTIONS – CHERRY STREET STATION 113+00 TO STATION 113+96
X9	CROSS SECTIONS – MILL STREET STATION 200+89 TO STATION 202+75
X10	CROSS SECTIONS – MILL STREET STATION 203+00 TO STATION 205+00
X11	CROSS SECTIONS – RAILROAD STREET STATION 205+25 TO STATION 207+11
X12	CROSS SECTIONS – RAILROAD STREET STATION 207+27 TO STATION 208+25
X13	CROSS SECTIONS – RAILROAD ALLEY STATION 301+95 TO STATION 302+70
X14	CROSS SECTIONS – RAILROAD ALLEY STATION 302+80 TO STATION 303+57
X15	CROSS SECTIONS – RAILROAD ALLEY STATION 303+70 TO STATION 304+50
X16	CROSS SECTIONS – RAILROAD ALLEY STATION 304+80 TO STATION 305+40
X17	CROSS SECTIONS – GARFIELD AVENUE STATION 601+02 TO STATION 601+82
X18	CROSS SECTIONS – GARFIELD AVENUE STATION 601+92 TO STATION 602+72
X19	CROSS SECTIONS – GARFIELD AVENUE STATION 602+82 TO STATION 603+82

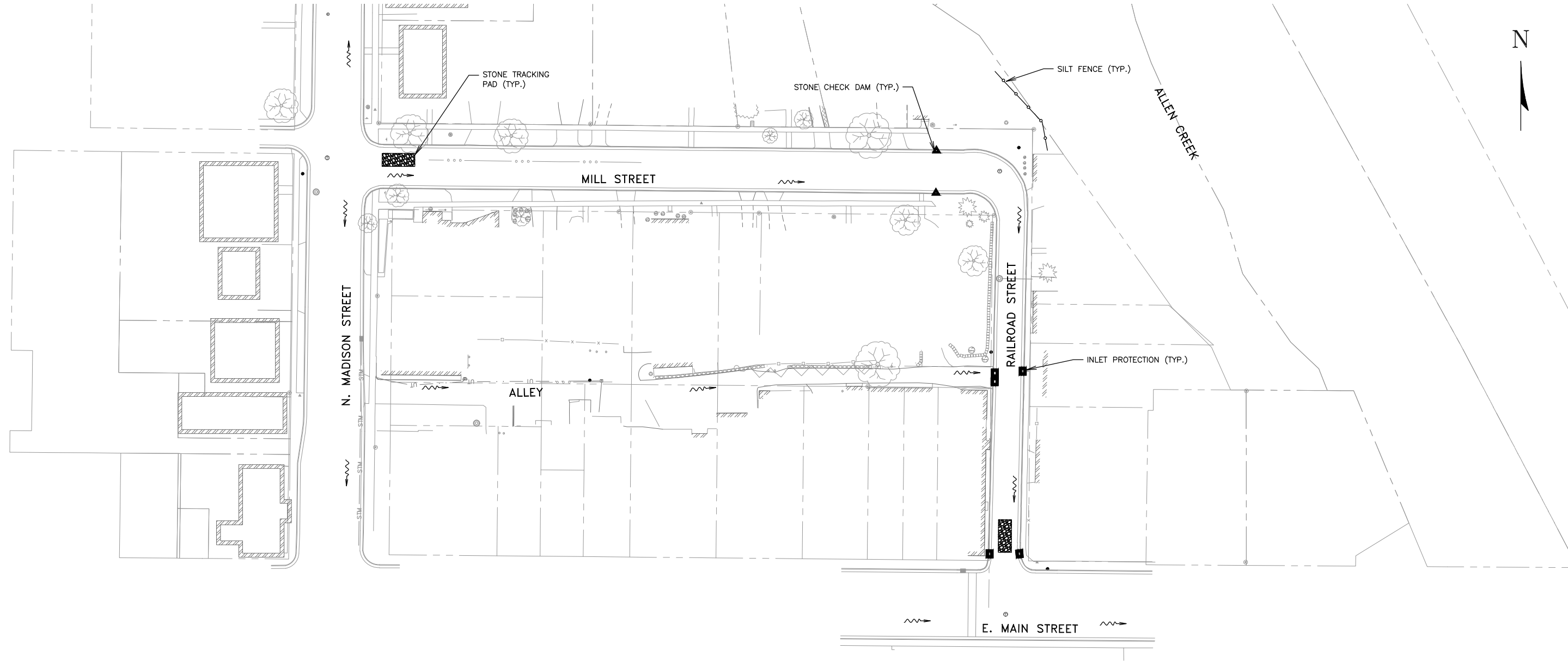


**EROSION CONTROL NOTES:**

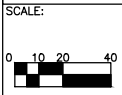
- LOCATIONS MARKED WITH "■" TO RECEIVE INLET FILTER PROTECTION DURING CONSTRUCTION. ALL NEW STREET INLETS MUST ALSO RECEIVE INLET FILTER PROTECTION.
- CONSTRUCT A STONE CHECK DAM IN GUTTER LINE AT ALL LOCATIONS MARKED WITH "▲"
- SURFACE FLOW DIRECTION IS INDICATED WITH
- SILT FENCE INSTALLATION IS INDICATED WITH
- POST WDNR CERTIFICATE OF PERMIT COVERAGE ON SITE AND MAINTAIN UNTIL CONSTRUCTION ACTIVITIES HAVE CEASED, THE SITE IS STABILIZED, AND A NOTICE OF TERMINATION IS FILED WITH WDNR.
- KEEP A COPY OF THE CURRENT EROSION CONTROL PLAN ON SITE THROUGHOUT THE DURATION OF THE PROJECT.
- SUBMIT PLAN REVISIONS OR AMENDMENTS TO THE WDNR AT LEAST 5 DAYS PRIOR TO FIELD IMPLEMENTATION.
- THE CONTRACTOR IS RESPONSIBLE FOR ROUTINE SITE INSPECTIONS AT LEAST ONCE EVERY 7 DAYS AND WITHIN 24 HOURS AFTER A RAINFALL EVENT OF 0.5 INCHES OR GREATER. KEEP INSPECTION REPORTS ON-SITE AND MAKE THEM AVAILABLE UPON REQUEST.
- INSPECT AND MAINTAIN ALL INSTALLED EROSION CONTROL PRACTICES UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED.
- WHEN POSSIBLE: PRESERVE EXISTING VEGETATION (ESPECIALLY ADJACENT TO SURFACE WATERS), MINIMIZE LAND-DISTURBING CONSTRUCTION ACTIVITY ON SLOPES OF 20% OR MORE, MINIMIZE SOIL COMPACTION, AND PRESERVE TOPSOIL.
- REFER TO THE WDNR STORMWATER CONSTRUCTION TECHNICAL STANDARDS AT [http://dnr.wi.gov/topic/stormwater/standards/const\\_standards.html](http://dnr.wi.gov/topic/stormwater/standards/const_standards.html).
- INSTALL PERIMETER EROSION CONTROLS AND ROCK TRACKING PAD CONSTRUCTION ENTRANCE(S) PRIOR TO ANY LAND-DISTURBING ACTIVITIES, INCLUDING CLEARING AND GRUBBING. USE WDNR TECHNICAL STANDARD STONE TRACKING PAD AND TIRE WASHING #1057 FOR ROCK CONSTRUCTION ENTRANCE(S).
- INSTALL INLET PROTECTION PRIOR TO LAND-DISTURBING ACTIVITIES IN THE CONTRIBUTING DRAINAGE AREA AND/OR IMMEDIATELY UPON INLET INSTALLATION. COMPLY WITH WDNR TECHNICAL STANDARD STORM DRAIN INLET PROTECTION FOR CONSTRUCTION SITES #1060.

- STAGE CONSTRUCTION GRADING ACTIVITIES TO MINIMIZE THE CUMULATIVE EXPOSED AREA. CONDUCT TEMPORARY GRADING FOR EROSION CONTROL PER WDNR TECHNICAL STANDARD TEMPORARY GRADING PRACTICES FOR EROSION CONTROL #1067.
- NOTIFY THE OWNER IF DEWATERING IS SCHEDULED TO OCCUR IN AREAS OF SOIL AND/OR GROUNDWATER CONTAMINATION, OR IF DEWATERING WILL OCCUR FROM A HIGH CAPACITY WELL (70 GPM OR MORE). DEWATER ONLY AFTER THE APPROPRIATE WDNR DEWATERING DISCHARGE PERMIT HAS BEEN OBTAINED.
- PROVIDE ANTI-SCOUR PROTECTION AND MAINTAIN NON-EROSIVE FLOW DURING DEWATERING. LIMIT PUMPING RATES TO EITHER (A) THE SEDIMENT BASIN/TRAP DESIGN DISCHARGE RATE, OR (B) THE BASIN DESIGN RELEASE RATE WITH THE CORRECTLY-FITTED HOSE AND GEOTEXTILE FILTER BAG. PERFORM DEWATERING OF ACCUMULATED SURFACE RUNOFF IN ACCORDANCE WITH WDNR TECHNICAL STANDARD DE-WATERING #1061.
- INSTALL AND MAINTAIN SILT FENCING PER WDNR TECHNICAL STANDARD SILT FENCE #1056. REMOVE SEDIMENT FROM BEHIND SILT FENCES AND SEDIMENT BARRIERS BEFORE SEDIMENT REACHES A DEPTH THAT IS EQUAL TO ONE-HALF OF THE FENCE AND/OR BARRIER HEIGHT.
- REPAIR BREAKS AND GAPS IN SILT FENCES AND BARRIERS IMMEDIATELY. REPLACE DECOMPOSING STRAW BALES (TYPICAL BALE LIFE IS 3 MONTHS). LOCATE, INSTALL, AND MAINTAIN STRAW BALES PER WDNR TECHNICAL STANDARD DITCH CHECKS #1062.
- INSTALL AND MAINTAIN FILTER SOCKS IN ACCORDANCE WITH WDNR TECHNICAL STANDARD INTERIM MANUFACTURED PERIMETER CONTROL AND SLOPE INTERRUPTION PRODUCTS #1071.
- IMMEDIATELY STABILIZE STOCKPILES AND SURROUND STOCKPILES AS NEEDED WITH SILT FENCE OR OTHER PERIMETER CONTROL IF STOCKPILES WILL REMAIN INACTIVE FOR 7 DAYS OR LONGER.
- IMMEDIATELY STABILIZE ALL DISTURBED AREAS THAT WILL REMAIN INACTIVE FOR 14 DAYS OR LONGER. BETWEEN SEPTEMBER 15 AND OCTOBER 15: STABILIZE WITH MULCH, TACKIFIER, AND A PERENNIAL SEED MIXED WITH WINTER WHEAT, ANNUAL OATS, OR ANNUAL RYE, AS APPROPRIATE FOR REGION AND SOIL TYPE. OCTOBER 15 THROUGH COLD WEATHER: STABILIZE WITH A POLYMER AND DORMANT SEED MIX, AS APPROPRIATE FOR REGION AND SOIL TYPE.
- STABILIZE AREAS OF FINAL GRADING WITHIN 7 DAYS OF REACHING FINAL GRADE.
- SWEEP/CLEAN UP ALL SEDIMENT/TRASH THAT MOVES OFF-SITE DUE TO CONSTRUCTION ACTIVITY OR STORM EVENTS BEFORE THE END OF THE SAME WORKDAY OR AS DIRECTED BY THE OWNER. SEPARATE SWEEPED MATERIALS (SOILS AND TRASH) AND DISPOSE OF APPROPRIATELY.
- THE CONTRACTOR IS RESPONSIBLE FOR CONTROLLING DUST PER WDNR TECHNICAL STANDARD DUST CONTROL ON CONSTRUCTION SITES #1068.

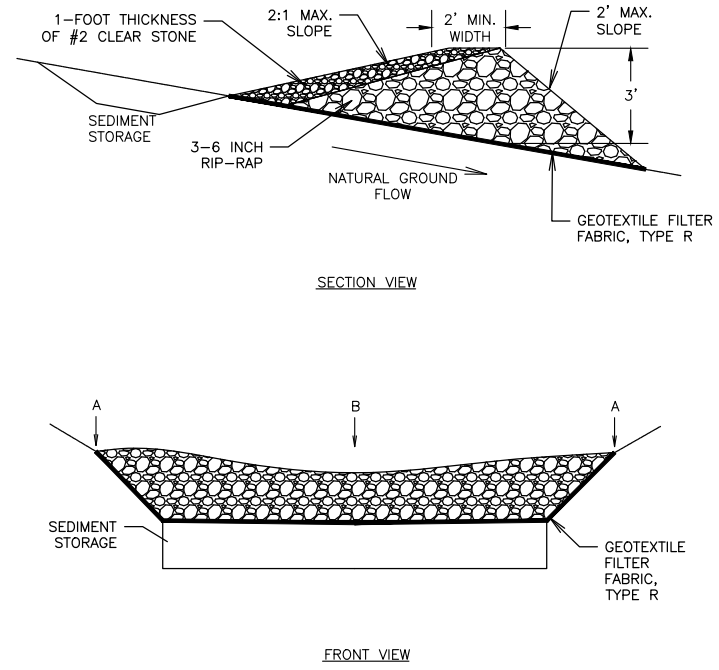
- PROPERLY DISPOSE OF ALL WASTE AND UNUSED BUILDING MATERIALS (INCLUDING GARBAGE, DEBRIS, CLEANING WASTES, OR OTHER CONSTRUCTION MATERIALS) AND DO NOT ALLOW THESE MATERIALS TO BE CARRIED BY RUNOFF INTO THE RECEIVING CHANNEL.
- FOR NON-CHANNELIZED FLOW ON DISTURBED OR CONSTRUCTED SLOPES, PROVIDE CLASS I, II OR III TYPE A EROSION CONTROL MATTING. SELECT EROSION MATTING FROM APPROPRIATE MATRIX IN WDOT'S WIDOT PRODUCT ACCEPTABILITY LIST (PAL); INSTALL AND MAINTAIN PER WDNR TECHNICAL STANDARD NON-CHANNEL EROSION MAT #1052.
- FOR CHANNELIZED FLOW ON DISTURBED OR CONSTRUCTED AREAS, PROVIDE CLASS I, II, OR III TYPE B EROSION CONTROL MATTING. SELECT EROSION MATTING FROM APPROPRIATE MATRIX IN WDOT'S WIDOT PRODUCT ACCEPTABILITY LIST (PAL); INSTALL AND MAINTAIN PER WDNR TECHNICAL STANDARD CHANNEL EROSION MAT #1053.



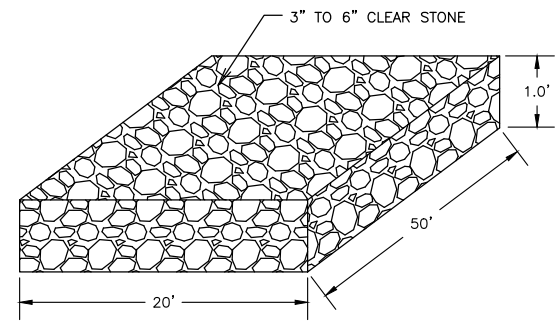
PROJECT NO.:	EV 124
DRAWING FILE:	EV124 MILL-RR SHEETS.DWG
DRAWN BY:	A.M.C.
CHECKED BY:	N.R.B.
DATE:	2-14-25
REVISIONS:	





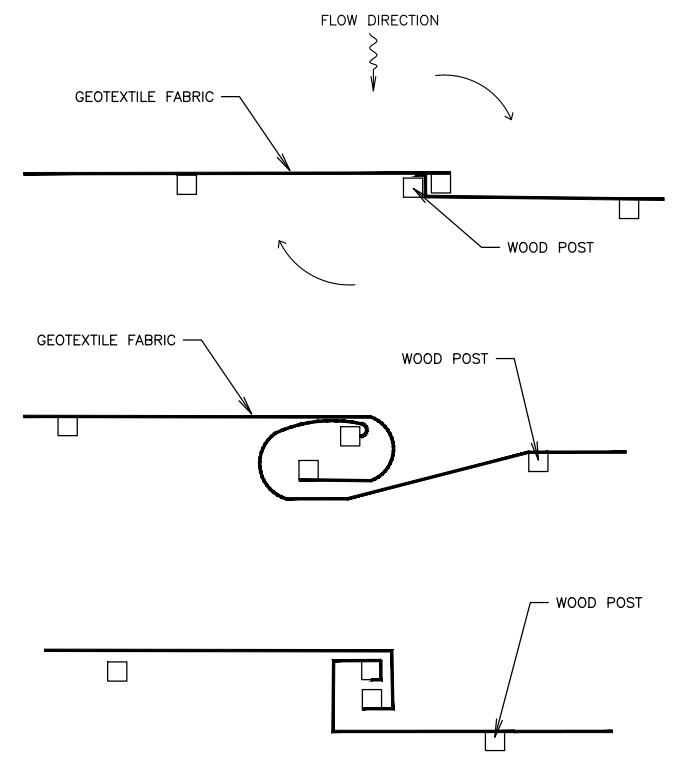


**DETAIL**  
STONE CHECK DAM

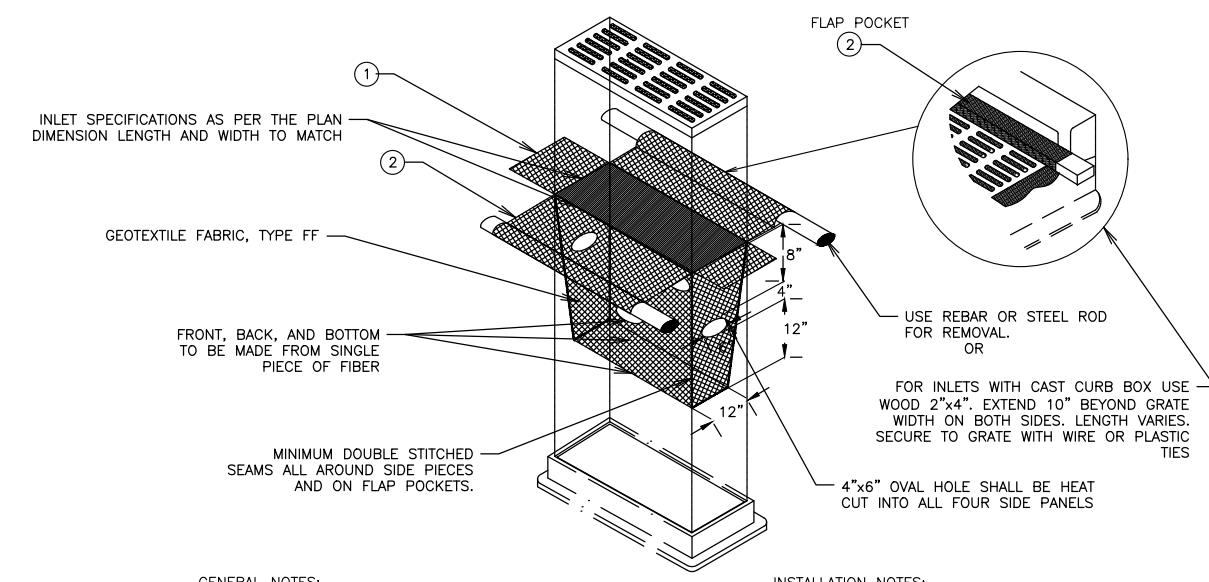


NOTE:  
- ON STREET SURFACES  
CRUSHED AGGREGATE BASE STONE  
SERVES AS TRACKING PAD.

**DETAIL**  
CLEAR STONE TRACKING PAD



**DETAIL**  
SEDIMENT FENCE JOINT

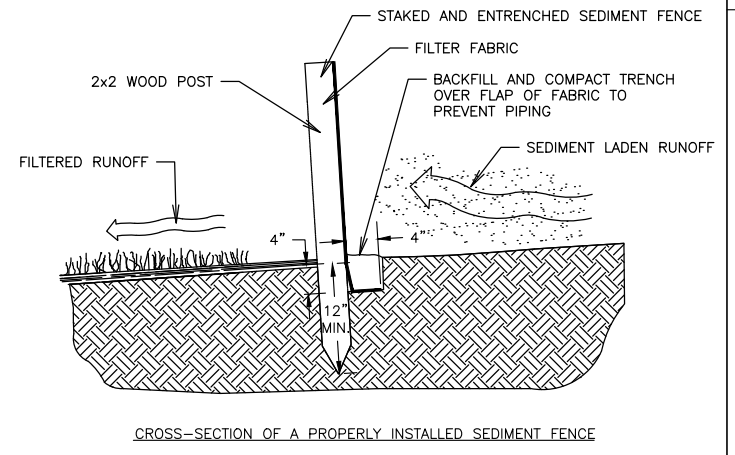


**GENERAL NOTES:**  
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

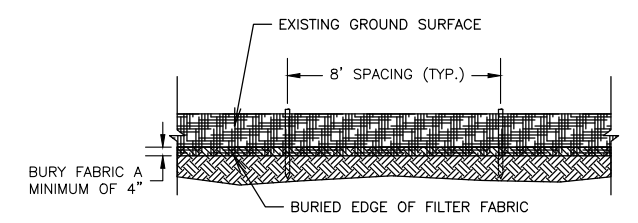
- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2x4.

**INSTALLATION NOTES:**  
DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 3 FT. MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.  
TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.  
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

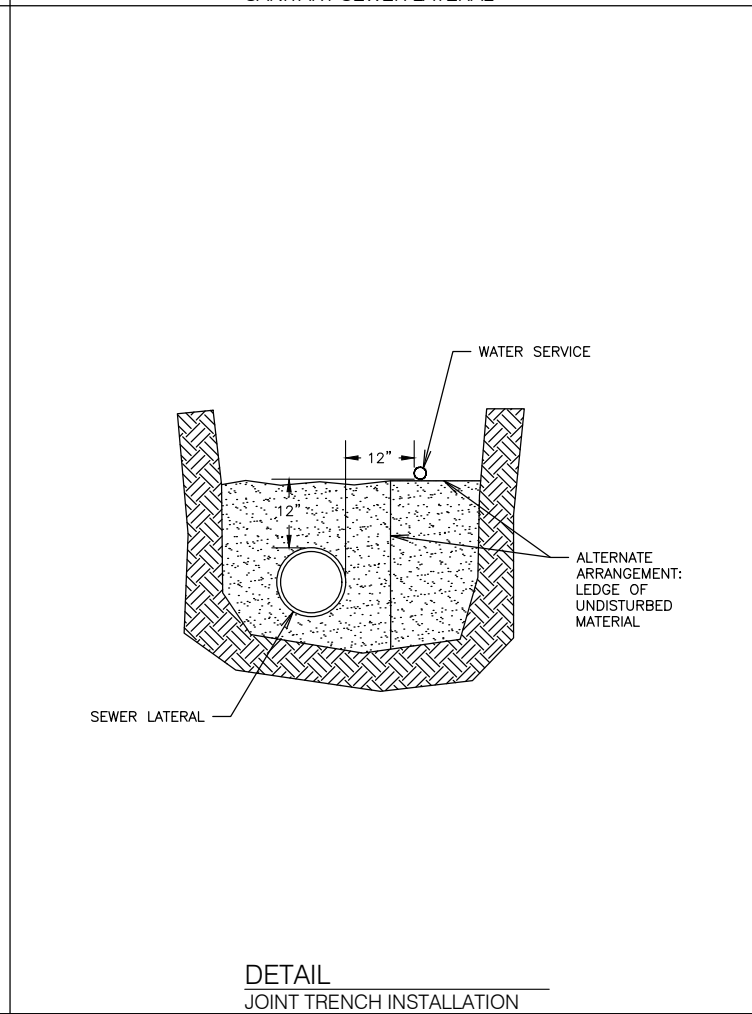
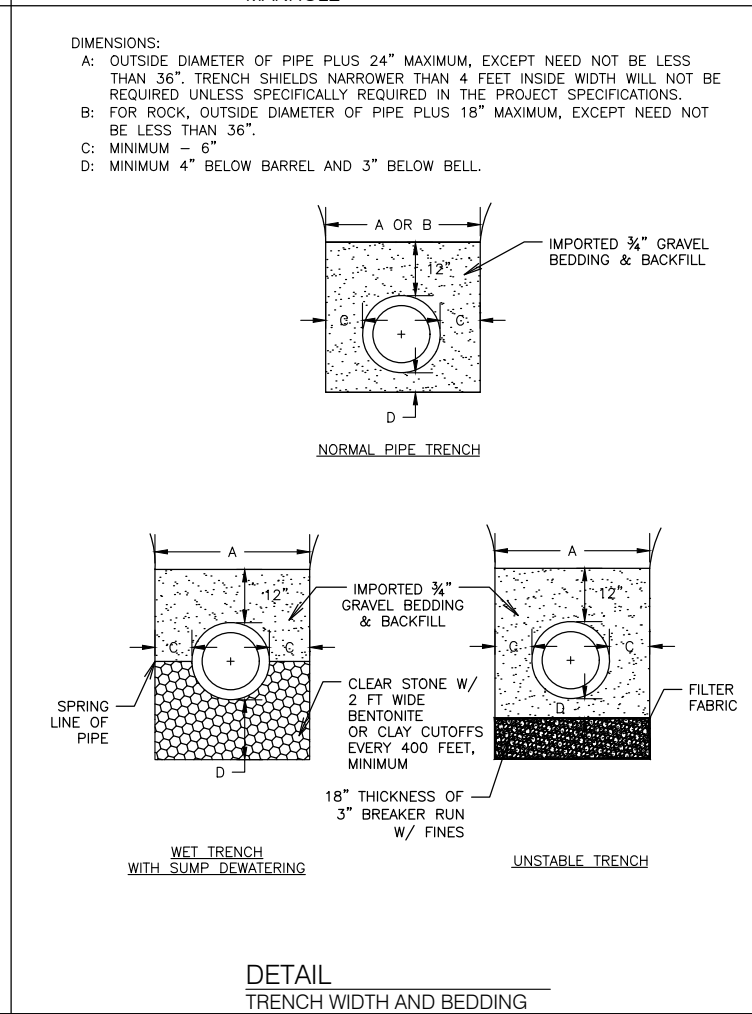
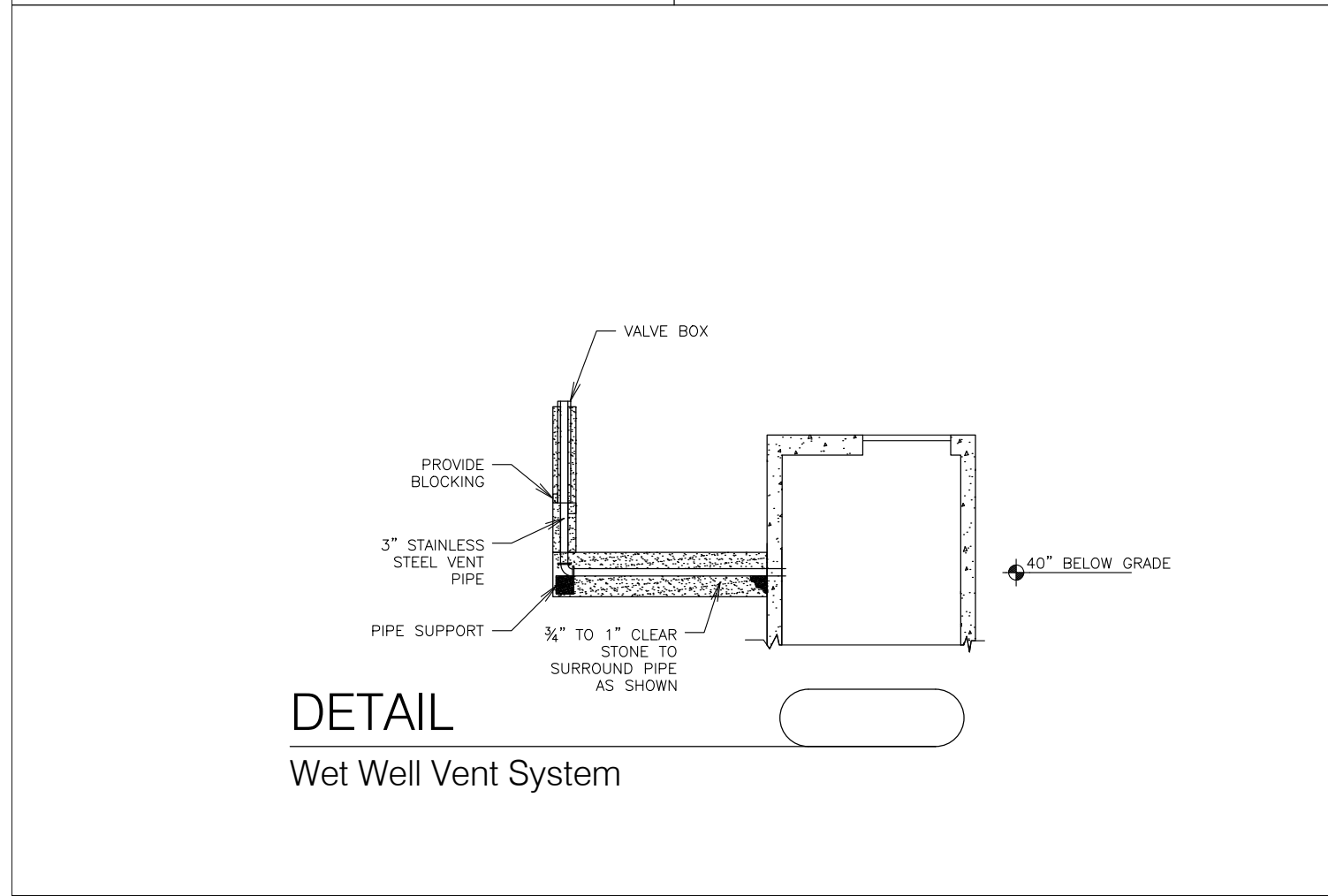
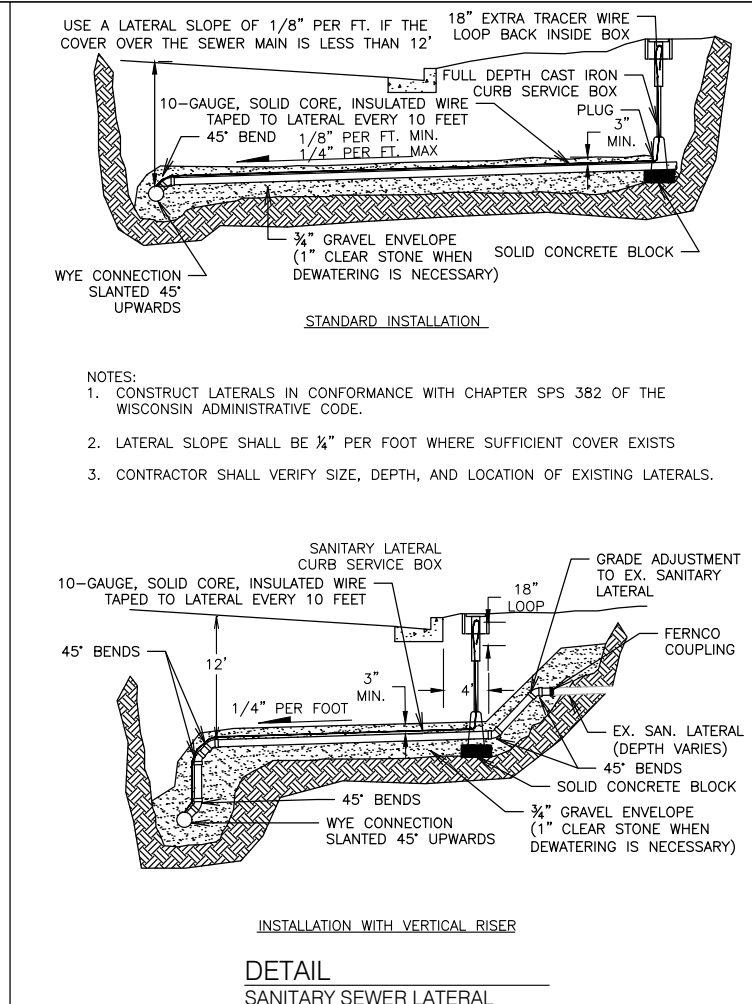
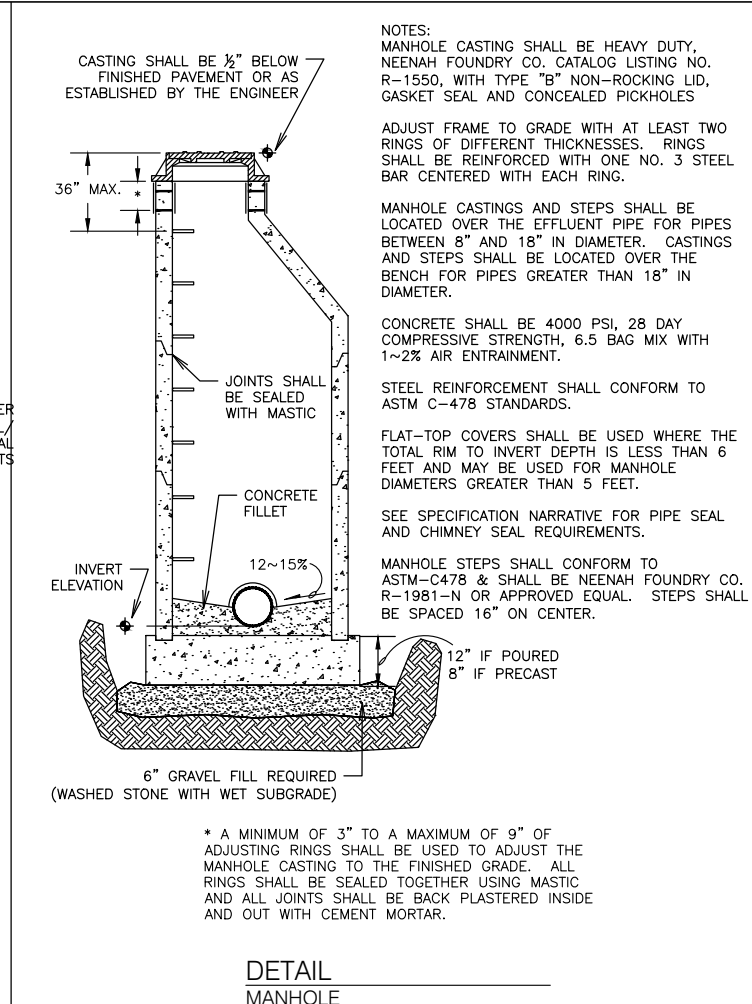
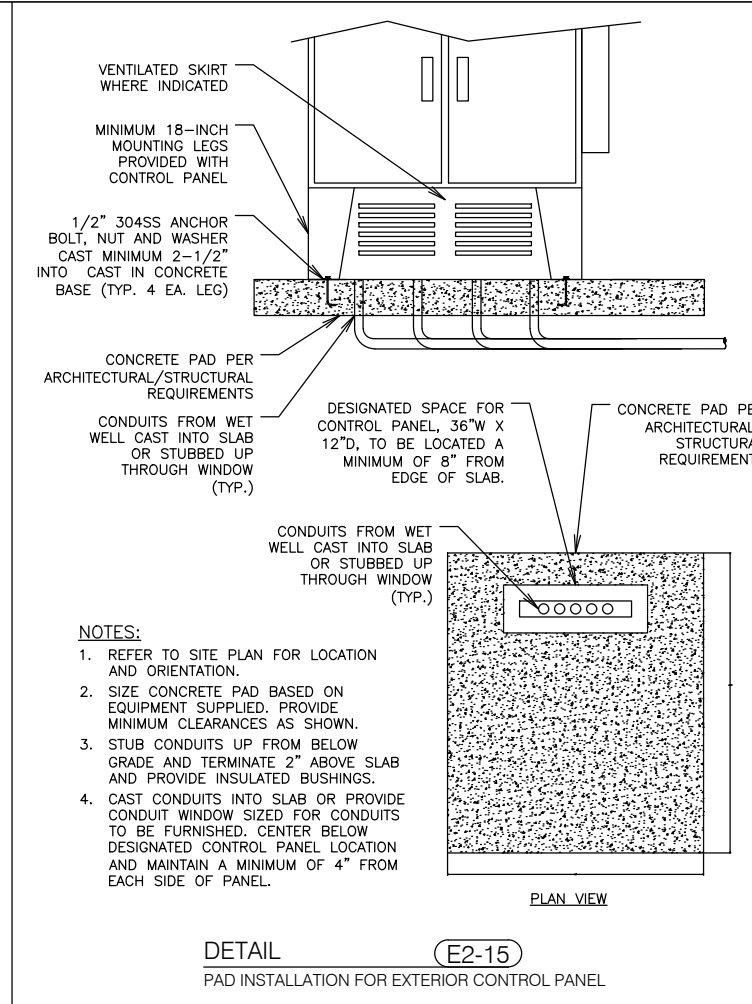
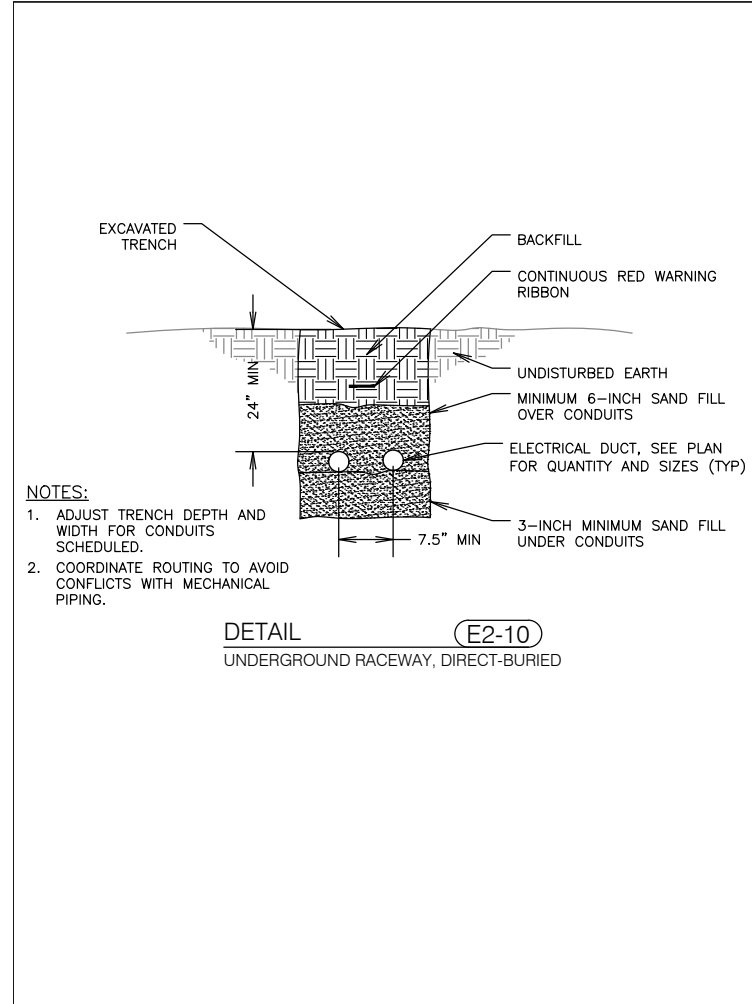
**DETAIL**  
INLET PROTECTION - TYPE D (W/ CURB BOX)



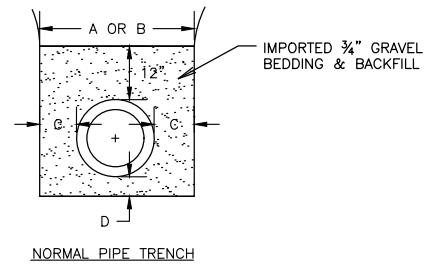
**CROSS-SECTION OF A PROPERLY INSTALLED SEDIMENT FENCE**



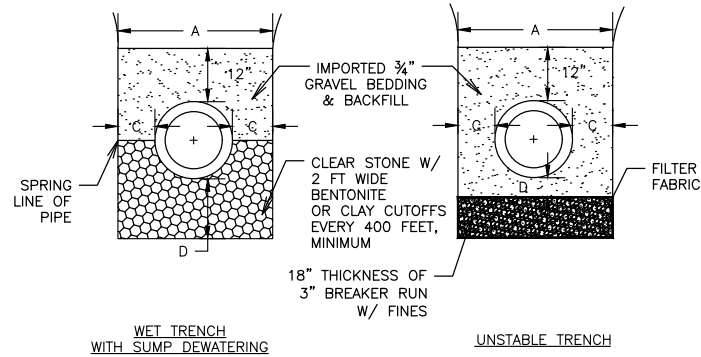
**DETAIL**  
SEDIMENT FENCE



DIMENSIONS:  
 A: OUTSIDE DIAMETER OF PIPE PLUS 24" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36". TRENCH SHIELDS NARROWER THAN 4 FEET INSIDE WIDTH WILL NOT BE REQUIRED UNLESS SPECIFICALLY REQUIRED IN THE PROJECT SPECIFICATIONS.  
 B: FOR ROCK, OUTSIDE DIAMETER OF PIPE PLUS 18" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36".  
 C: MINIMUM - 6"  
 D: MINIMUM 4" BELOW BARREL AND 3" BELOW BELL.



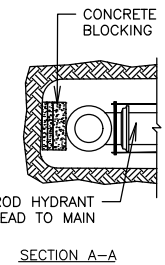
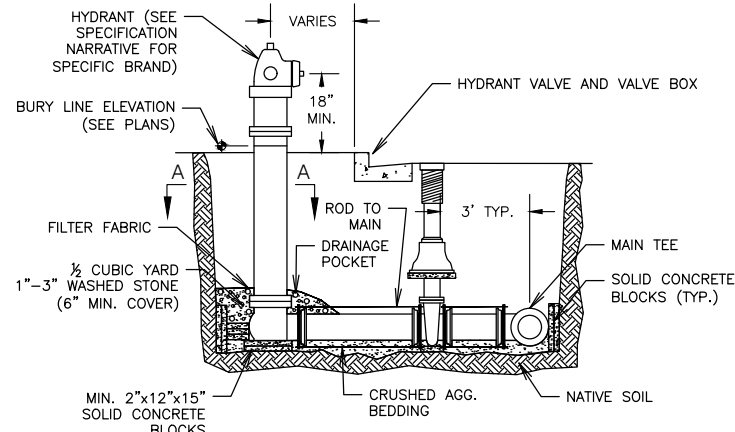
NORMAL PIPE TRENCH



WET TRENCH WITH SUMP DEWATERING

UNSTABLE TRENCH

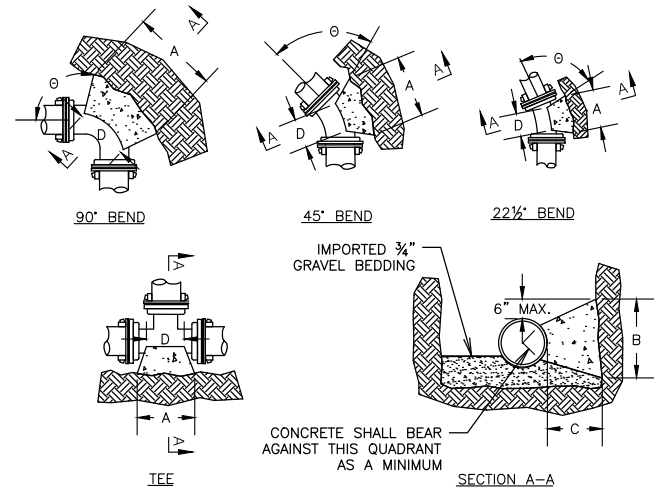
DETAIL  
TRENCH WIDTH AND BEDDING



SECTION A-A

NOTES:  
 - WOOD BLOCKING MAY NOT BE USED. ONLY SOLID CONCRETE BLOCKS ARE ALLOWED.  
 - THE HYDRANT AND HYDRANT VALVE SHALL BE CONNECTED TO THE MAIN TEE BY RODDING IN ACCORDANCE WITH DETAIL "OFFSET AND RODDING", OR BY MEGALUGS.  
 - THE DISTANCE BETWEEN THE HYDRANT AND THE MAIN WILL VARY. OFFSET DISTANCES ARE MARKED ON THE PLANS.  
 - WHERE THE HYDRANT IS INSTALLED AT THE HIGH POINT OF THE WATER MAIN ON MAINS 10 INCHES IN DIAMETER AND LARGER, THE CONTRACTOR SHALL TIP THE MAIN TEE UPWARDS 45 DEGREES AND USE A 45 DEGREE FITTING TO ALLOW AIR TO ESCAPE FROM THE MAIN.

DETAIL  
HYDRANT SETTING



WOOD BLOCKING MAY NOT BE USED. ONLY SOLID CONCRETE BLOCKS ARE ALLOWED.  
 DIMENSION "D" SHALL BE AS LARGE AS POSSIBLE, BUT THE CONCRETE SHALL NOT INTERFERE WITH THE MECHANICAL JOINTS.  
 DIMENSION "C" SHALL BE AT LEAST 6 INCHES, AND LARGE ENOUGH TO MAKE THE "theta" ANGLE EQUAL TO OR GREATER THAN 45 DEGREES WITH THE DIMENSION "A" AS SHOWN ON THE TABLE, OR GREATER, AND WITH DIMENSION "D" AS LARGE AS POSSIBLE.

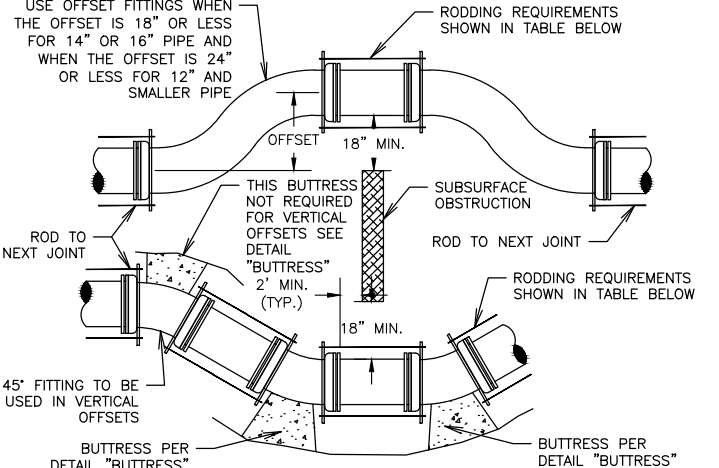
CONCRETE SHALL BE CLASS "CC". SEE SECTION 03301.

PIPE SIZE	BUTTRUSS DIMENSIONS							
	TEES		22.5° BEND		45° BEND		90° BEND	
	A	B	A	B	A	B	A	B
6	1'-3"	1'-0"	1'-0"	1'-0"	1'-0"	1'-0"	1'-4"	1'-2"
8	1'-6"	1'-4"	1'-0"	1'-0"	1'-4"	1'-2"	1'-10"	1'-6"
10	2'-0"	1'-4"	1'-4"	1'-10"	1'-10"	2'-8"	2'-3"	
14	2'-6"	1'-10"	1'-8"	2'-6"	2'-4"	3'-10"	2'-10"	
18	3'-0"	2'-4"	2'-0"	3'-3"	2'-10"	5'-0"	3'-4"	
22	3'-4"	2'-10"	2'-4"	4'-0"	3'-3"	6'-4"	3'-10"	
30	4'-3"	3'-6"	3'-0"	5'-4"	3'-10"	8'-0"	4'-8"	

\* = FOR TEE THIS WILL BE THE BRANCH PIPE

DIMENSIONS IN THE TABLE ARE BASED ON A WATER PRESSURE OF 150 PSI AND SOIL RESISTANCE OF 200 LBS/SQ.FT.

DETAIL  
BUTTRUSS

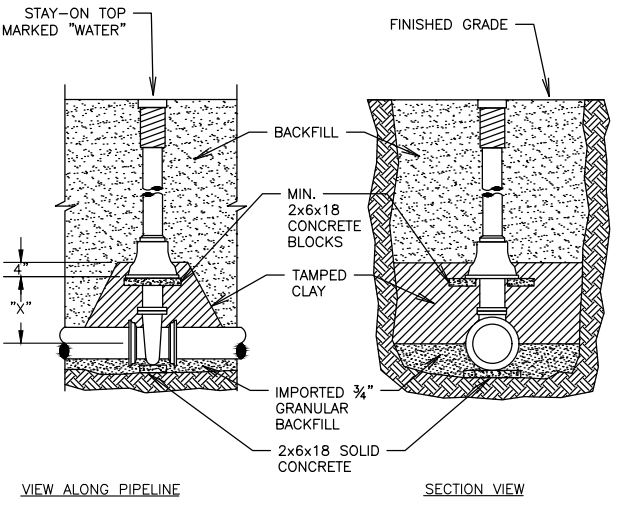


NOMINAL PIPE SIZE	RODS NO.	RODS DIA.	STRAP SIZE	BOLT DIA.	WASHER SIZE
6	3	3/8"	1/2 x 2	3/8"	1/2 x 3 x 5
8	4	3/8"	1/2 x 2	3/8"	1/2 x 3 x 5
10	4	3/8"	1/2 x 2 1/2	1"	1/2 x 3 x 5
12	4	3/8"	1/2 x 2 1/2	1"	1/2 x 3 x 5
14	4	3/8"	1/2 x 2 1/2	1"	1/2 x 3 x 5

ALL DIMENSIONS IN THIS TABLE ARE IN INCHES

NOTES:  
 - RODS AND WASHERS TO BE ASTM A-575 MERCHANT QUALITY 0.17-0.24 CARBON. NUTS TO BE AMERICAN STANDARD HEAVY, NOT PRESSED.  
 - THE RODS, BOLTS, NUTS, BANDS AND WASHERS TO BE FURNISHED AND ASSEMBLED BY THE CONTRACTOR.  
 - ALL STEEL MATERIAL TO BE GALVANIZED OR THOROUGHLY COATED WITH ENGINEER APPROVED COATING.  
 - OFFSET FITTINGS REQUIRE CONTINUOUS RODDING IN ALL POSITIONS.  
 - VERTICAL OFFSETS SHALL NOT CREATE A HIGH POINT IN THE WATER MAIN. VERTICAL OFFSETS REQUIRE THE SAME RODDING AND BUTTRUSSING AS SHOWN ABOVE.  
 - MEGALUG RESTRAINTS MAY BE USED IN LIEU OF RODDING.

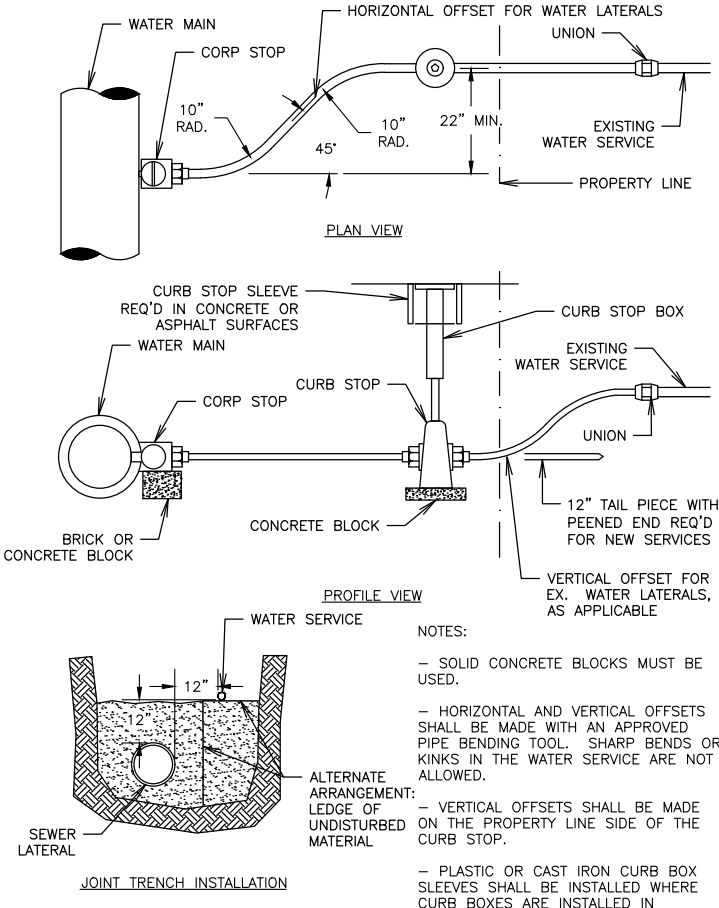
DETAIL  
OFFSET AND RODDING



PIPE DIA., INCHES	6	8	10	12	14	16
"X" DIMENSION, INCHES	12	13	17	21	25	30

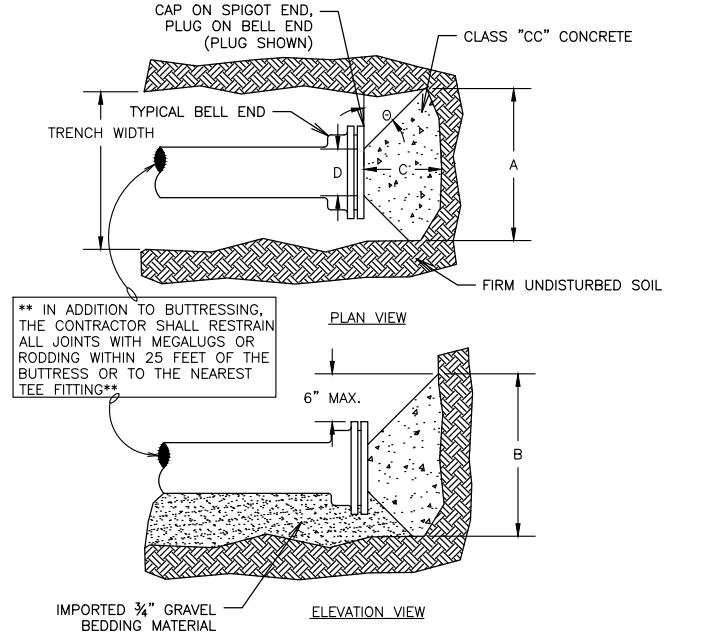
NOTES:  
 - SOLID CONCRETE BLOCKS MUST BE USED.  
 - VALVES SHALL BE SECURED WITH RODDING OR MEGALUGS TO THE NEAREST "TEE" FITTING OR TO THE FIRST JOINT CONNECTING A FULL SECTION OF WATER MAIN PIPE. SEE RODDING DETAIL "OFFSET AND RODDING".

DETAIL  
VALVE BOX SETTING



NOTES:  
 - SOLID CONCRETE BLOCKS MUST BE USED.  
 - HORIZONTAL AND VERTICAL OFFSETS SHALL BE MADE WITH AN APPROVED PIPE BENDING TOOL. SHARP BENDS OR KINKS IN THE WATER SERVICE ARE NOT ALLOWED.  
 - VERTICAL OFFSETS SHALL BE MADE ON THE PROPERTY LINE SIDE OF THE CURB STOP.  
 - PLASTIC OR CAST IRON CURB BOX SLEEVES SHALL BE INSTALLED WHERE CURB BOXES ARE INSTALLED IN CONCRETE OR ASPHALT SURFACES.

DETAIL  
WATER SERVICE INSTALLATION



DIA.	BUTTRUSS DIMENSIONS			
	A	B	C	D
6"	1'-3"	1'-0"		
8"	1'-8"	1'-6"		
10"	2'-0"	1'-8"		
12"	2'-5"	1'-10"		
16"	3'-4"	2'-4"		
20"	4'-3"	2'-10"		
24"	5'-2"	3'-4"		
30"	6'-9"	4'-0"		

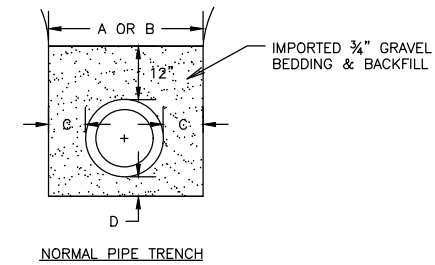
SEE NOTES ABOVE

NOTES:  
 - DIMENSION "C" SHALL BE LARGE ENOUGH TO MAKE ANGLE theta EQUAL TO OR GREATER THAN 45°. DIMENSION "D" EQUALS APPROX. I.D. OF PIPE, LESS 2 INCHES. CONTRACTOR SHALL PROTECT THE MECH. JOINT BOLTS FROM THE CONCRETE BUTTRUSS.  
 - BUTTRUSS DIMENSIONS ARE BASED UPON A SOIL RESISTANCE OF 2 TONS PER SQ. FT. AND A WATER PRESSURE OF 150 P.S.I.

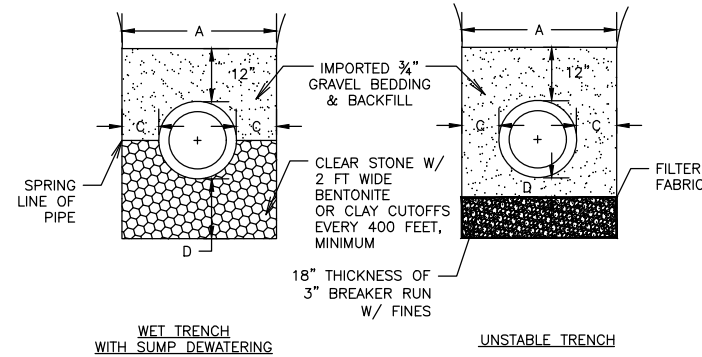
DETAIL  
BUTTRUSS FOR DEAD ENDS

**DIMENSIONS:**

- A: OUTSIDE DIAMETER OF PIPE PLUS 24" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36". TRENCH SHIELDS NARROWER THAN 4 FEET INSIDE WIDTH WILL NOT BE REQUIRED UNLESS SPECIFICALLY REQUIRED IN THE PROJECT SPECIFICATIONS.
- B: FOR ROCK, OUTSIDE DIAMETER OF PIPE PLUS 18" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36".
- C: MINIMUM - 6"
- D: MINIMUM 4" BELOW BARREL AND 3" BELOW BELL.

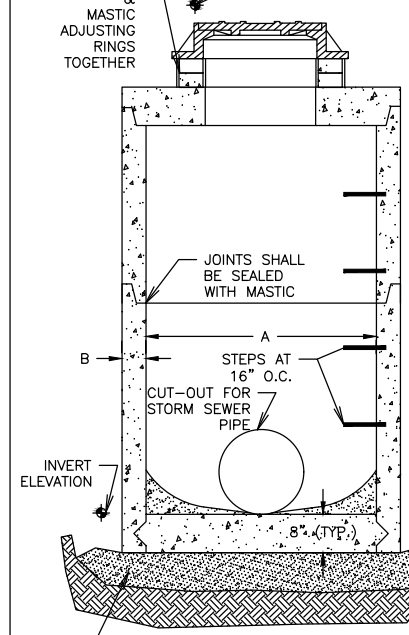


NORMAL PIPE TRENCH



DETAIL  
TRENCH WIDTH AND BEDDING

CASTING SHALL BE 3/4" BELOW FINISHED PAVEMENT OR AS ESTABLISHED BY THE ENGINEER.

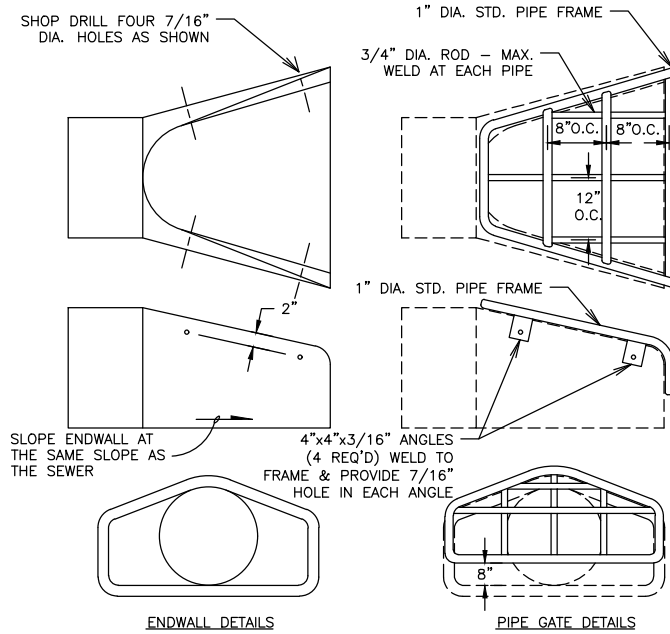


STORM MANHOLE DIMENSIONS

MANHOLE SIZE	DIMENSION	
	A	B (MIN.)
48"	48"	5"
60"	60"	6"
72"	72"	7"
84"	84"	7"
96"	96"	9"

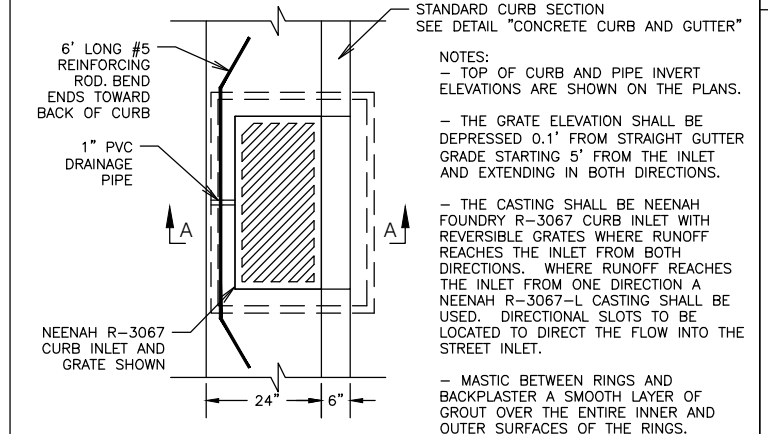
- NOTES:**
- CONCRETE SHALL BE 4000 PSI, 28 DAY COMPRESSIVE STRENGTH, 6.5 BAG MIX WITH 1~2% AIR ENTRAINMENT.
  - MANHOLE STEPS SHALL CONFORM TO ASTM-C478 & SHALL BE NEENAH FOUNDRY R-1981-N OR APPROVED EQUAL. STEPS SHALL BE SPACED 16" ON CENTER.
  - ADJUST FRAME TO GRADE WITH AT LEAST TWO PRECAST CONCRETE RINGS OF DIFFERENT THICKNESSES. RINGS SHALL BE REINFORCED WITH ONE NO.3 STEEL BAR CENTERED WITHIN EACH RING.
  - A MINIMUM OF 3" TO A MAXIMUM OF 9" OF ADJUSTING RINGS SHALL BE USED TO ADJUST THE MANHOLE CASTING TO THE FINISHED GRADE. ALL RINGS SHALL BE SEALED TOGETHER USING MASTIC AND ALL JOINTS SHALL BE BACK PLASTERED INSIDE AND OUT WITH CEMENT MORTAR.
  - MANHOLE CASTING SHALL BE HEAVY DUTY, NEENAH FOUNDRY R-1550-A, WITH TYPE "B" NON-ROCKING LID, GASKET SEAL AND CONCEALED PICK HOLES.
  - MANHOLE INLET CASTING SHALL BE NEENAH FOUNDRY R-2050 WITH TYPE "C" NON-ROCKING GRATE.
  - MANHOLE-TYPE CURB INLET CASTING SHALL BE NEENAH FOUNDRY R-3067 WITH REVERSIBLE GRATES WHERE RUNOFF REACHES THE INLET FROM BOTH DIRECTIONS. WHERE RUNOFF REACHES THE INLET FROM ONE DIRECTION A NEENAH R-3067-L CASTING SHALL BE USED. DIRECTIONAL SLOTS TO BE LOCATED TO DIRECT THE FLOW INTO THE CURB INLET.
  - A 2x3 LID OPENING IS REQUIRED FOR MANHOLE-TYPE CURB INLETS. ADJUSTING RINGS SHOULD THEN BE LIMITED TO 6" MAX.

DETAIL  
STORM SEWER MANHOLE AND INLET

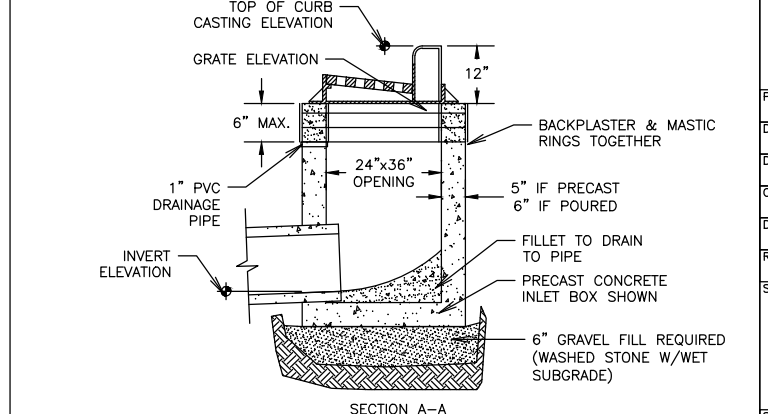


DETAIL  
ENDWALLS

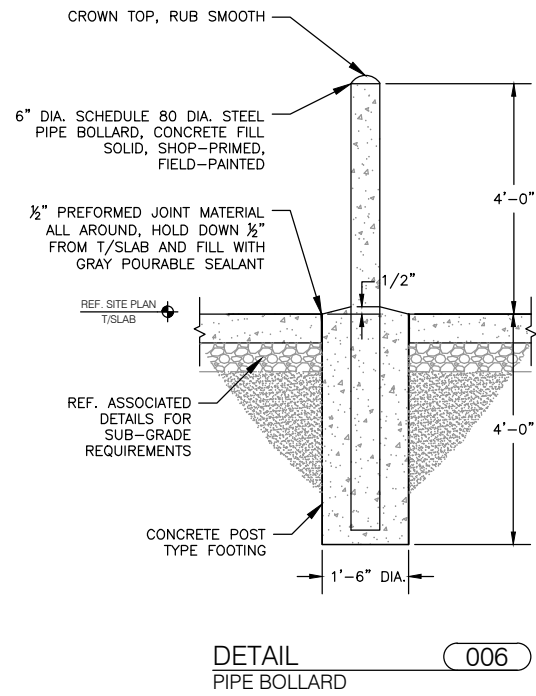
- NOTES:**
- THE CONTRACTOR SHALL BOLT THE PIPE GATE TO THE CONCRETE ENDWALL WITH FOUR 3/8"x6" MACHINE BOLTS WITH NUTS ON INSIDE WALL.
- PAINTING SPECIFICATIONS:**
- THE PIPE GATE SHALL RECEIVE THE FOLLOWING PREPARATION & PAINTING. THE FIRST COAT SHALL BE RUS-OLEUM X-60 RED BARE METAL PRIMER OR APPROVED EQUAL. THE SECOND COAT SHALL BE RUS-OLEUM 960 ZINC CHROMATE PRIMER OR APPROVED EQUAL. THE THIRD COAT SHALL BE RUS-OLEUM 1282 HIGH GLOSS METAL FINISH OR APPROVED EQUAL.
- PREPARATION STEPS:**
1. BARE METAL SURFACES - TREAT WITH THE THREE-COAT PAINTING SYSTEM LISTED AFTER A THOROUGH SCRAPING, WIRE BRUSHING & CLEANING.
  2. EACH COAT OF PAINT SHALL BE APPLIED OVER THE ENTIRE GATE SURFACE.
  3. ALLOW 24-48 HOURS DRYING TIME AT 60° OR ABOVE BETWEEN COATS.



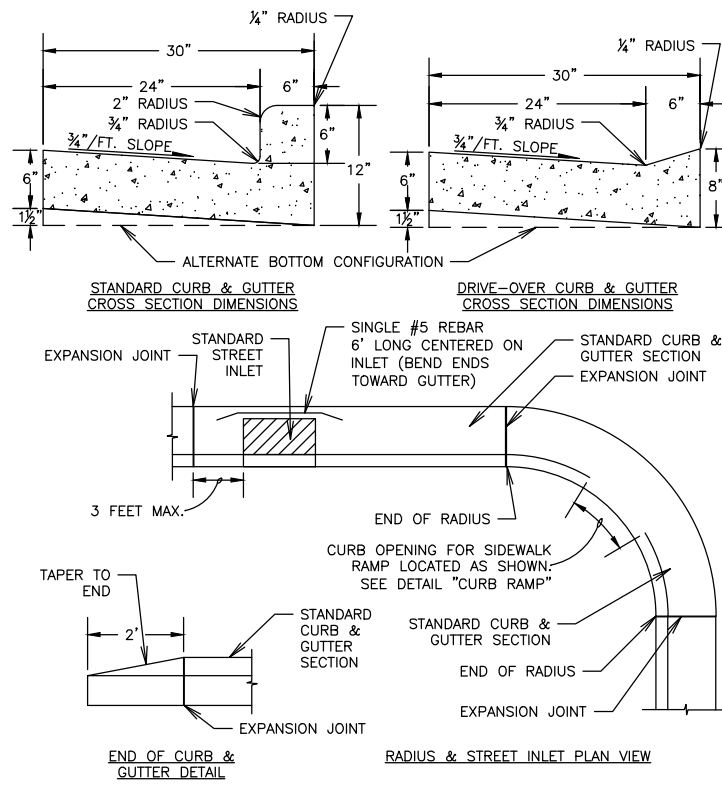
DETAIL  
RECTANGULAR CURB INLET



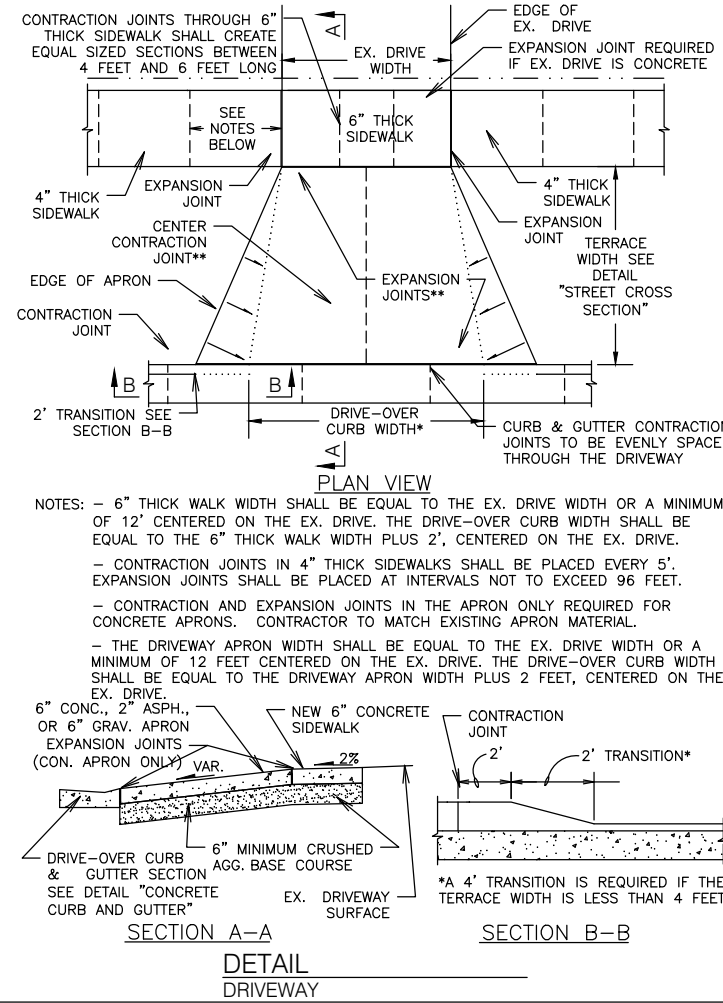
DETAIL  
RECTANGULAR CURB INLET



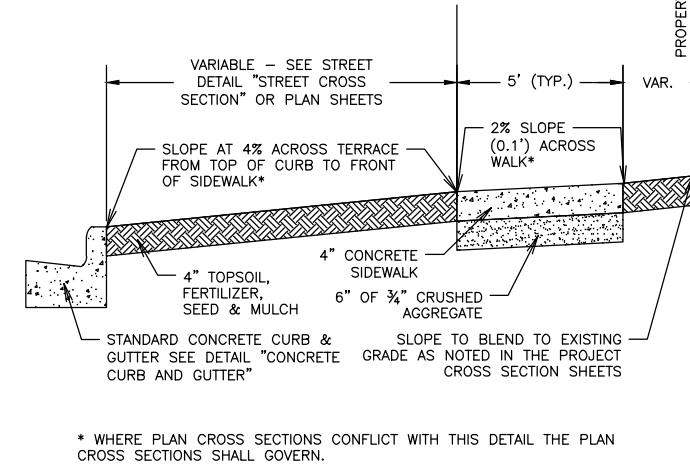
DETAIL PIPE BOLLARD 006



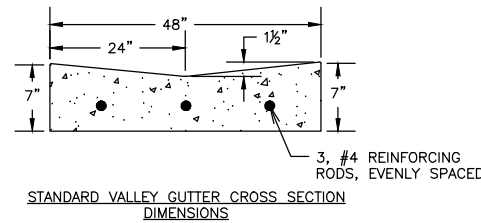
DETAIL CONCRETE CURB AND GUTTER



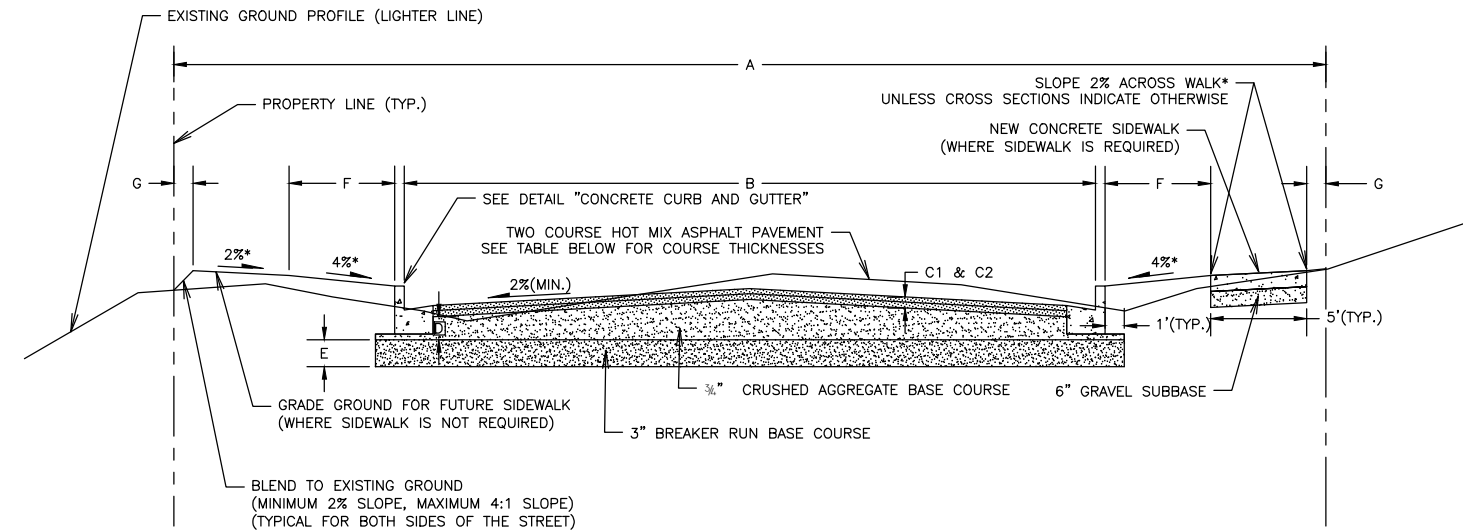
DETAIL DRIVEWAY



DETAIL SIDEWALK - TERRACE SECTION



DETAIL VALLEY GUTTER



STANDARD STREET DETAIL DIMENSIONS

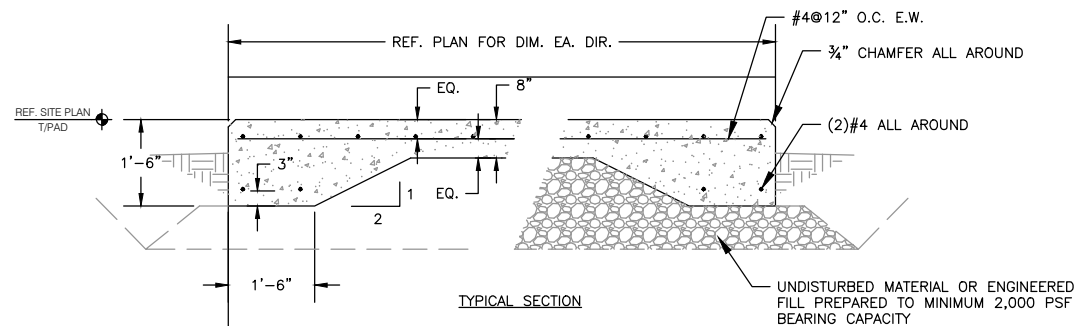
STREET NAME	A RIGHT OF WAY WIDTH	B CURB FACE TO CURB FACE WIDTH	C1 LOWER COURSE THICKNESS	C2 SURFACE COURSE THICKNESS	D C.A.B.C. THICKNESS*	E 3" BREAKER RUN B.C. THICKNESS	F TERRACE WIDTH	G BACK OF WALK TO PROP. LINE
CHERRY STREET	66'	33'	1 1/4" (2025)	1 1/2" (2026)	6"	9" MIN.	VARIES	VARIES
FRANCES STREET	66'	33'	1 1/4" (2025)	1 1/2" (2026)	6"	9" MIN.	VARIES	VARIES
MILL STREET	66'	33'	1 1/4" (2025)	1 1/2" (2026)	6"	9" MIN.	VARIES	VARIES
RAILROAD STREET	66'	VARIES	1 1/4" (2025)	1 1/2" (2026)	6"	9" MIN.	VARIES	VARIES
ALLEYS	VARIES	N/A	N/A	3" (2025)	6"	6" MIN.	N/A	N/A
CHURCH ST. PARKING	VARIES	N/A	N/A	3" (2025)	6"	6" MIN.	N/A	N/A

\*WHERE PLAN CROSS SECTIONS CONFLICT WITH THIS DETAIL, THE PLAN CROSS SECTION SHALL GOVERN.

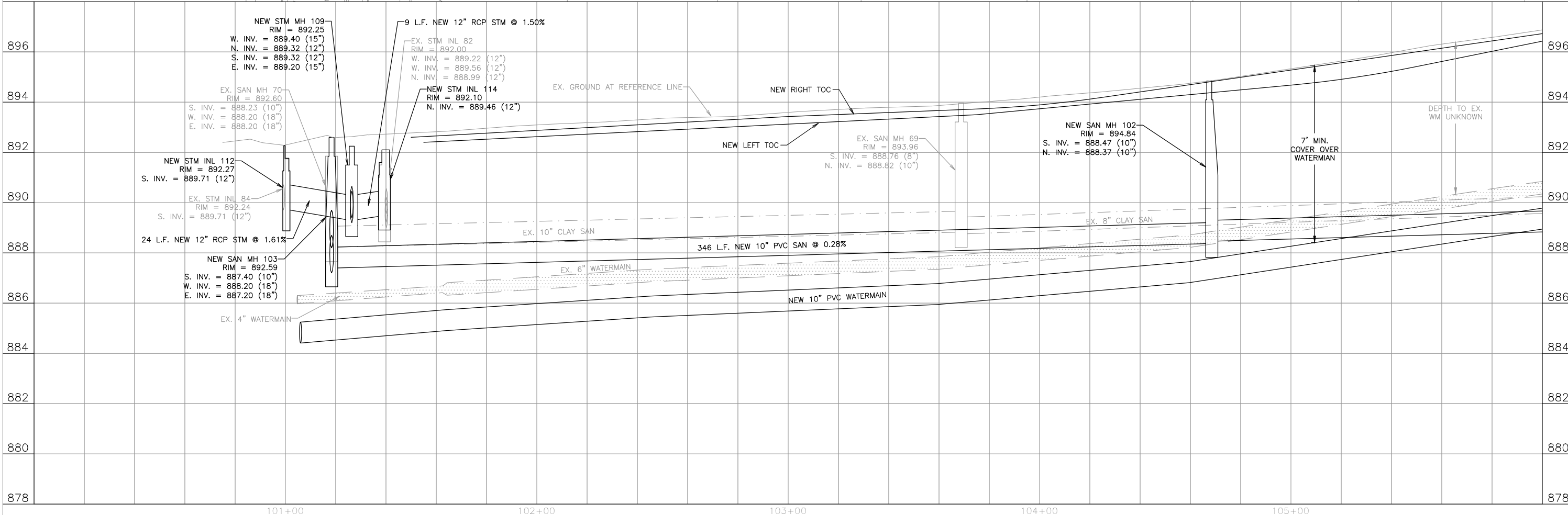
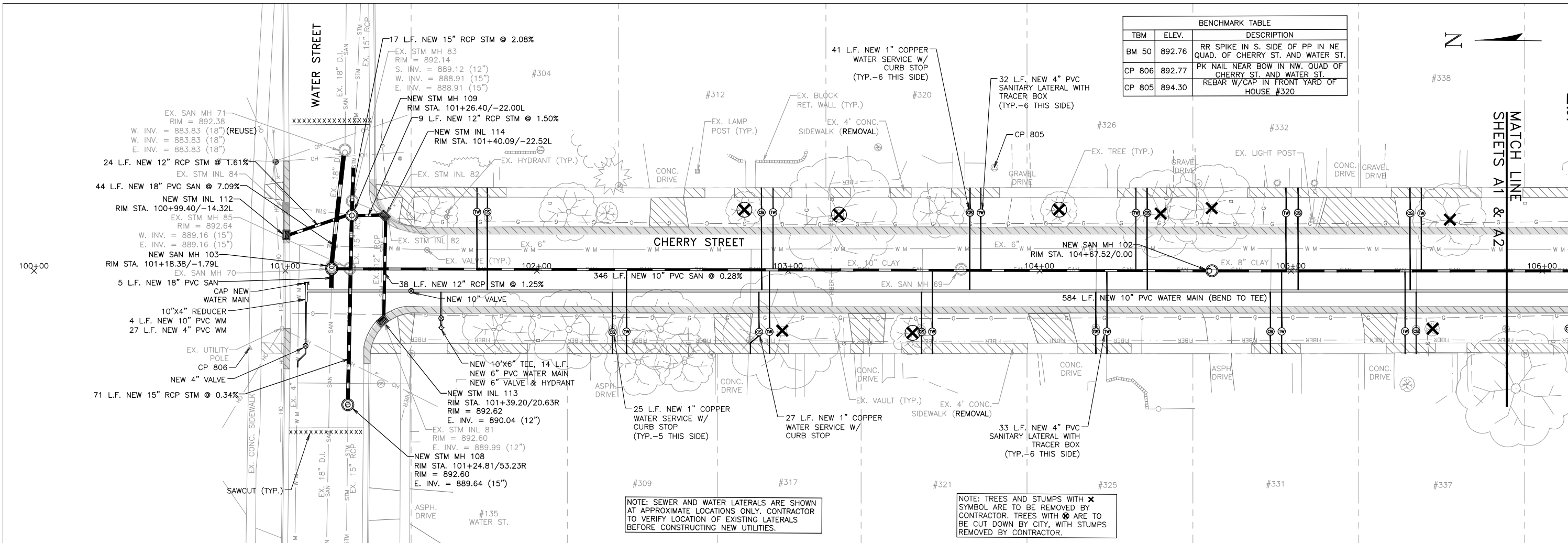
NOTES:  
THE CROWN OF THE ROAD SHALL BE CREATED USING THE 3/4" CRUSHED AGGREGATE BASE COURSE. THE THICKNESS SHOWN IS THE MINIMUM THICKNESS REQUIRED AS MEASURED AT THE CONCRETE CURB & GUTTER SECTION.

THE 3" BREAKER RUN BASE COURSE THICKNESS MAY NEED TO BE INCREASED DEPENDING UPON SUBGRADE CONDITIONS.

DETAIL STREET CROSS SECTION



DETAIL THICKENED EDGE EQUIPMENT SLAB



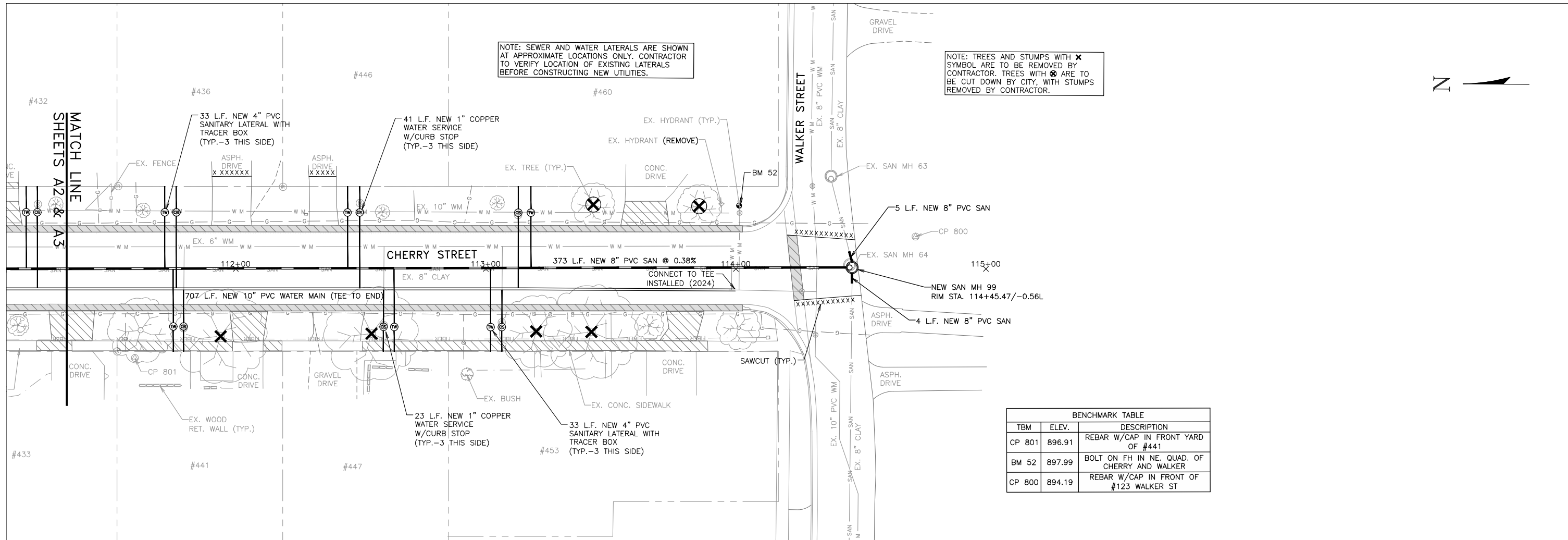
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Station 100+00 To Station 106+00



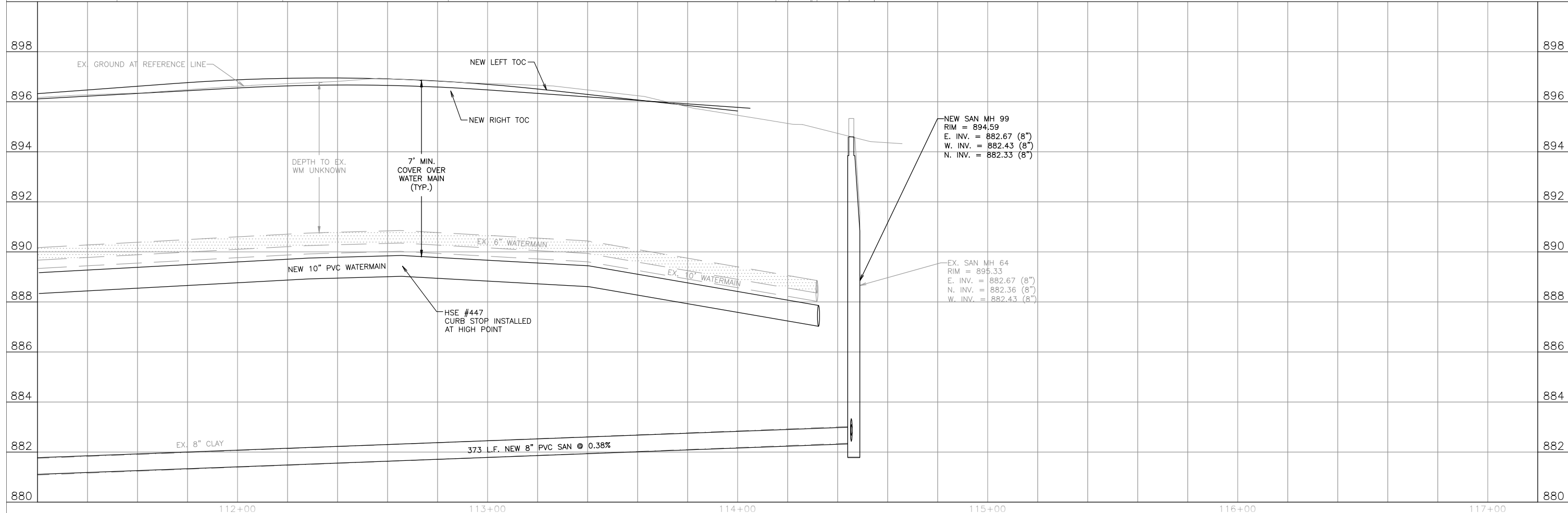




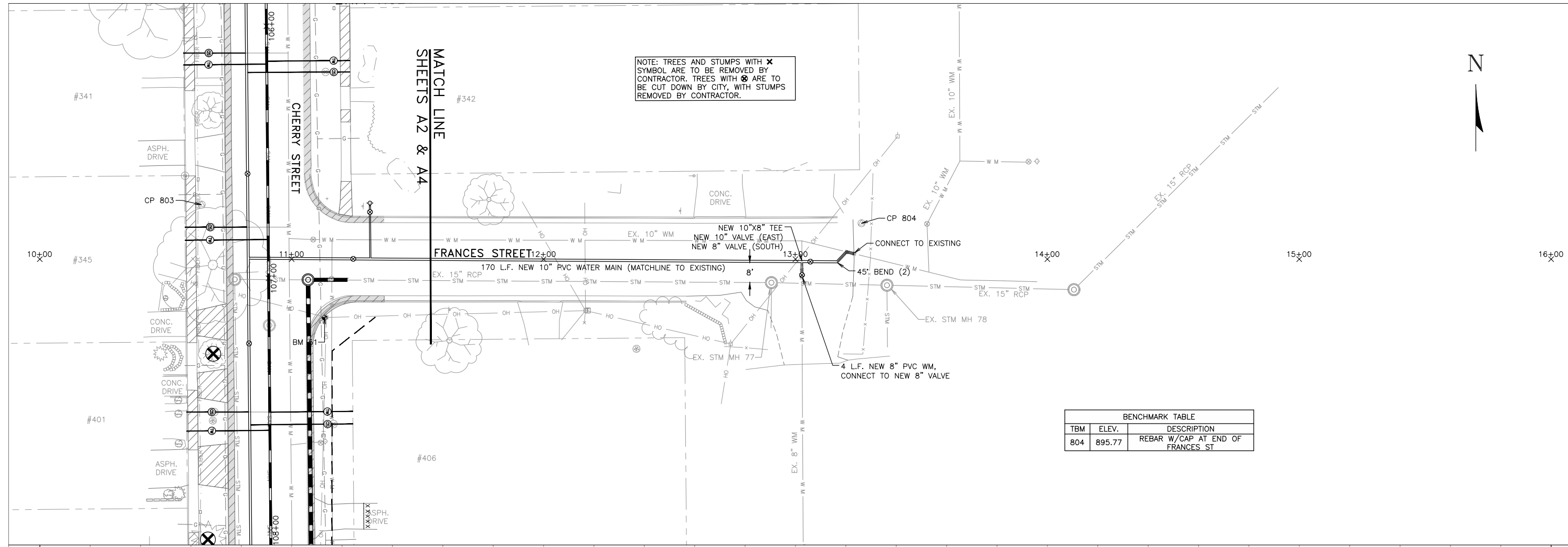
NOTE: SEWER AND WATER LATERALS ARE SHOWN AT APPROXIMATE LOCATIONS ONLY. CONTRACTOR TO VERIFY LOCATION OF EXISTING LATERALS BEFORE CONSTRUCTING NEW UTILITIES.

NOTE: TREES AND STUMPS WITH X SYMBOL ARE TO BE REMOVED BY CONTRACTOR. TREES WITH O ARE TO BE CUT DOWN BY CITY, WITH STUMPS REMOVED BY CONTRACTOR.

BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION
CP 801	896.91	REBAR W/CAP IN FRONT YARD OF #441
BM 52	897.99	BOLT ON FH IN NE. QUAD. OF CHERRY AND WALKER
CP 800	894.19	REBAR W/CAP IN FRONT OF #123 WALKER ST

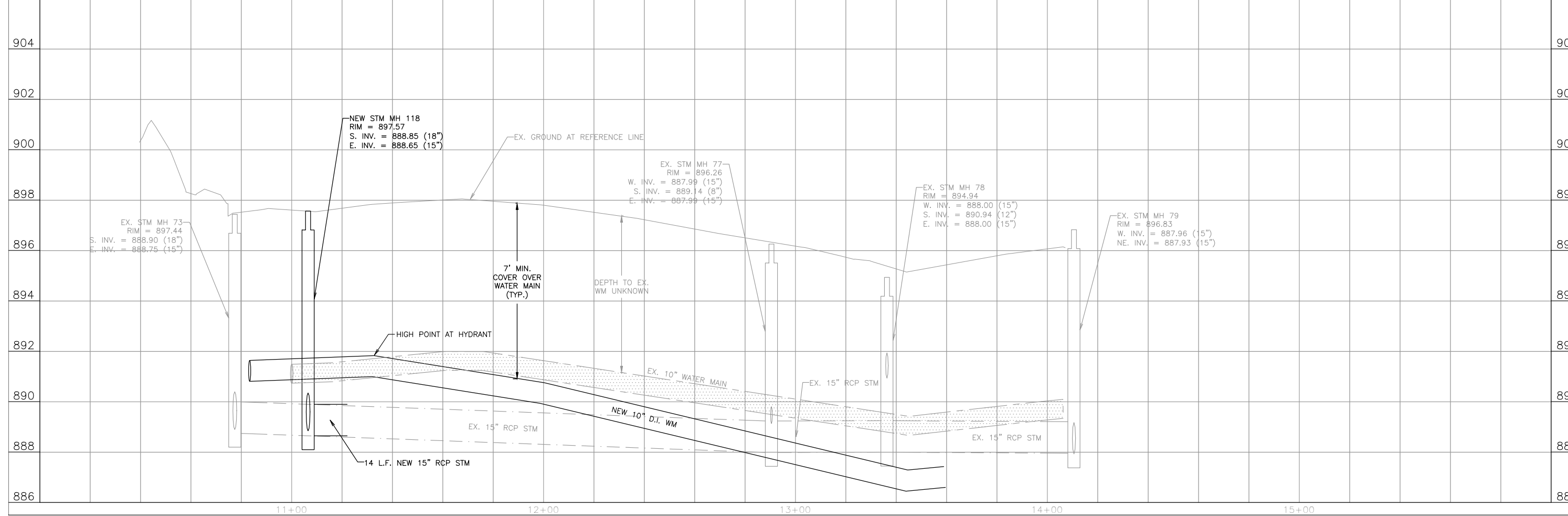






NOTE: TREES AND STUMPS WITH X SYMBOL ARE TO BE REMOVED BY CONTRACTOR. TREES WITH O ARE TO BE CUT DOWN BY CITY, WITH STUMPS REMOVED BY CONTRACTOR.

BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION
804	895.77	REBAR W/CAP AT END OF FRANCES ST

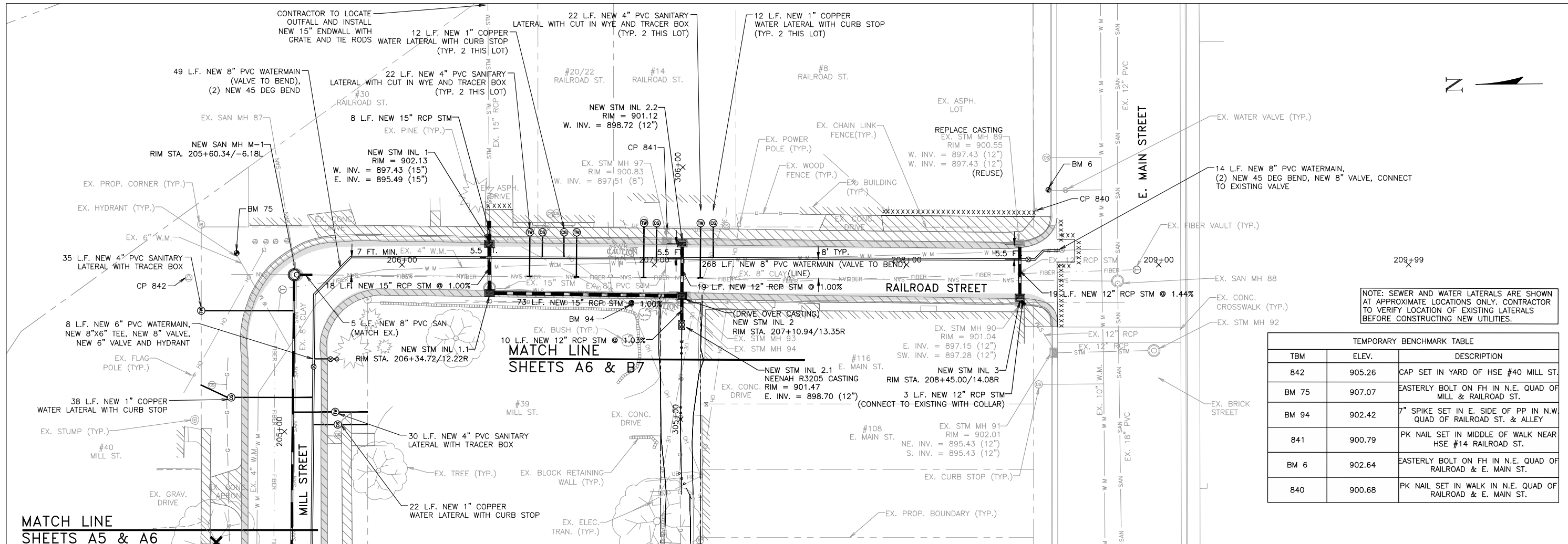


PLAN & PROFILE  
FRANCES STREET  
Station 10+00 To Station 16+00

2024 STREET AND UTILITY IMPROVEMENTS  
Cherry Street, Mill Street, and Railroad Street  
City of Evansville, Wisconsin

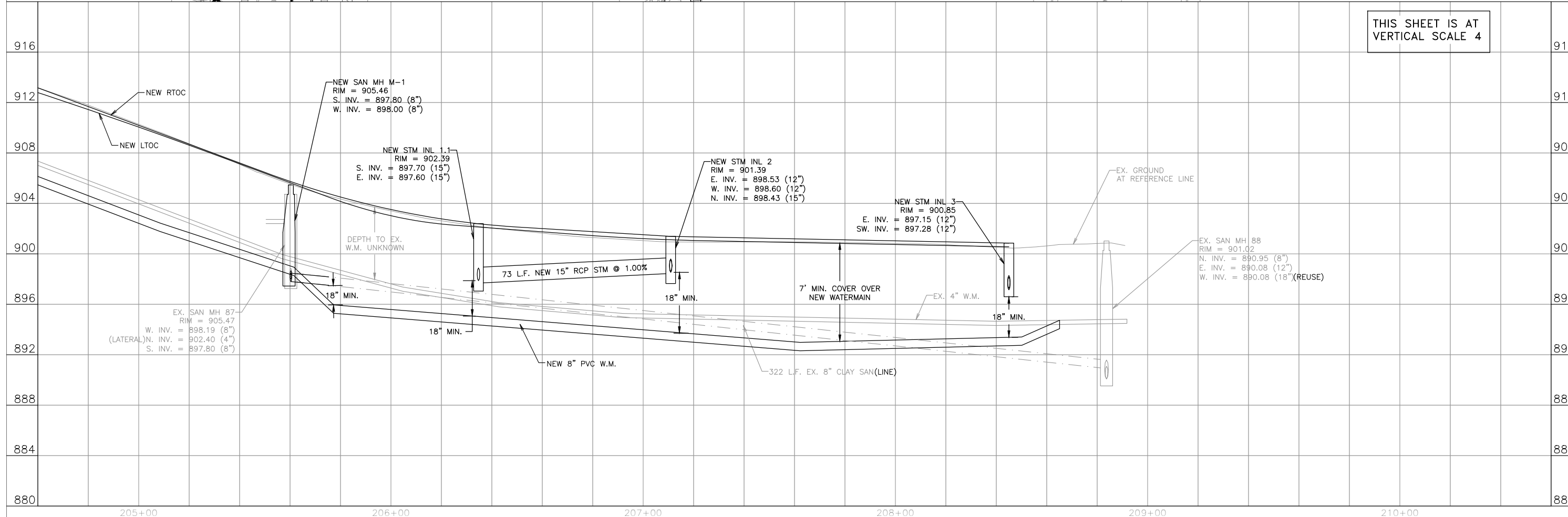
PROJECT NO.: EV 124  
DRAWING FILE: EV124 CHERRY SHEETS.DWG  
DRAWN BY: A.M.C.  
CHECKED BY: N.R.B.  
DATE: 2-14-25  
REVISIONS:  
SCALE: HORIZONTAL 1"=20'  
VERTICAL 1"=2'  
SHEET:



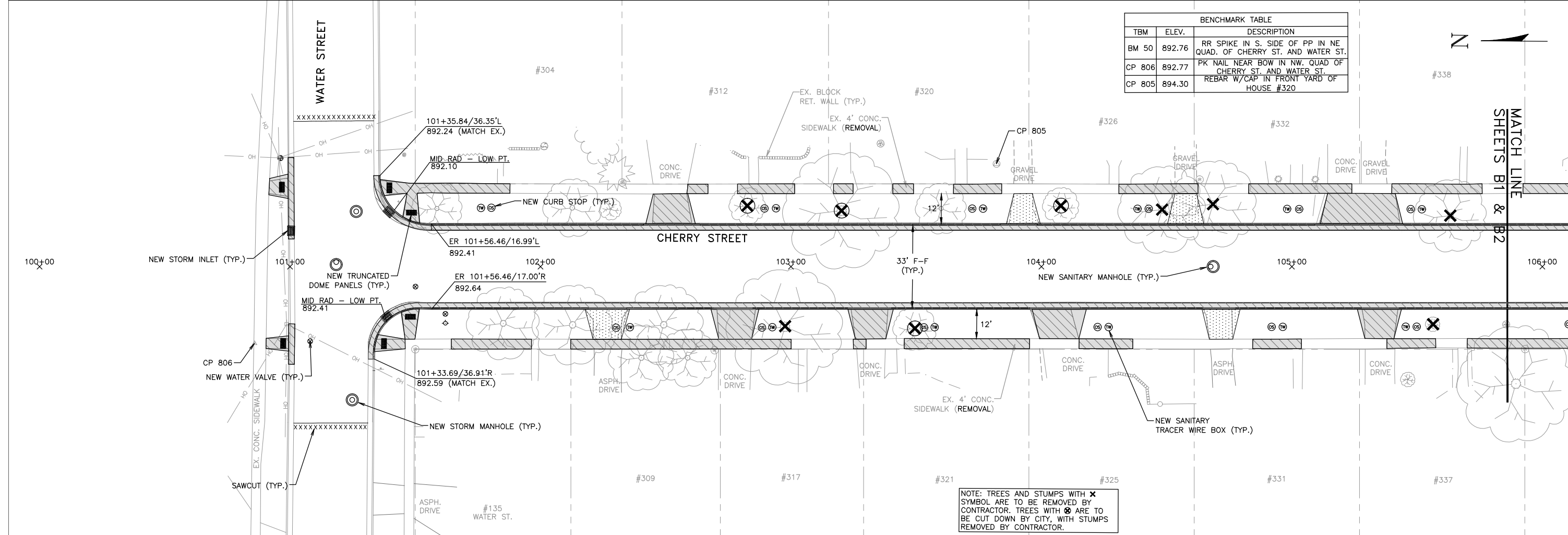


NOTE: SEWER AND WATER LATERALS ARE SHOWN AT APPROXIMATE LOCATIONS ONLY. CONTRACTOR TO VERIFY LOCATION OF EXISTING LATERALS BEFORE CONSTRUCTING NEW UTILITIES.

TEMPORARY BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION
842	905.26	CAP SET IN YARD OF HSE #40 MILL ST.
BM 75	907.07	EASTERLY BOLT ON FH IN N.E. QUAD OF MILL & RAILROAD ST.
BM 94	902.42	7" SPIKE SET IN E. SIDE OF PP IN N.W. QUAD OF RAILROAD ST. & ALLEY
841	900.79	PK NAIL SET IN MIDDLE OF WALK NEAR HSE #14 RAILROAD ST.
BM 6	902.64	EASTERLY BOLT ON FH IN N.E. QUAD OF RAILROAD & E. MAIN ST.
840	900.68	PK NAIL SET IN WALK IN N.E. QUAD OF RAILROAD & E. MAIN ST.

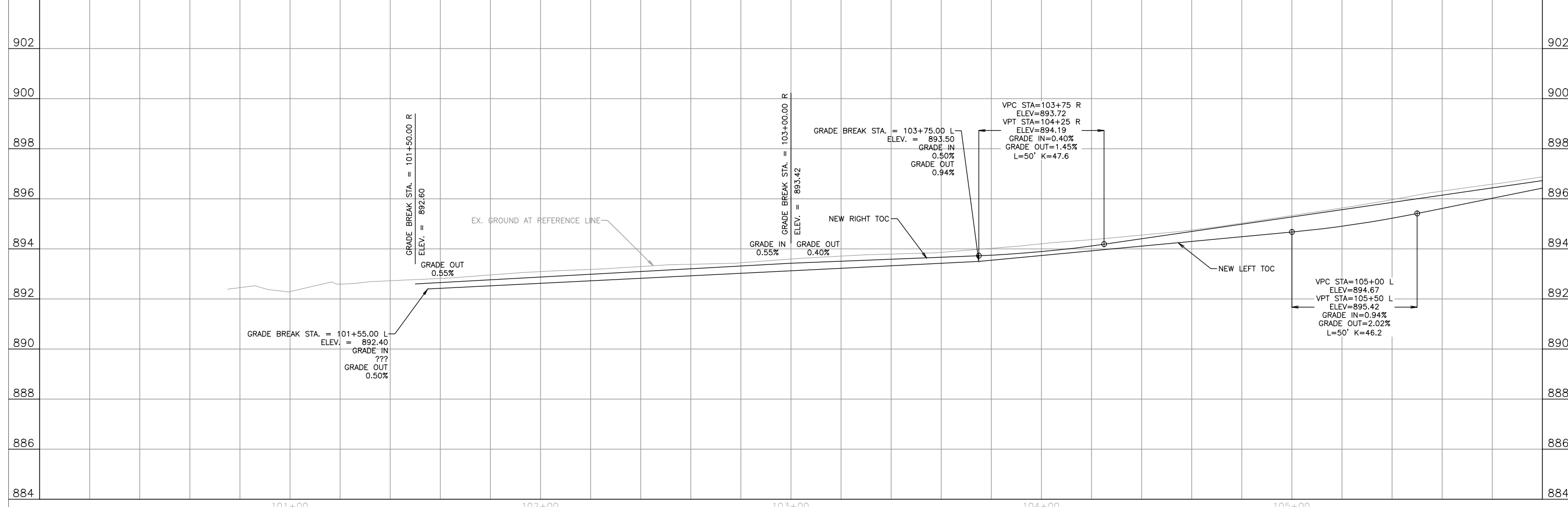


THIS SHEET IS AT VERTICAL SCALE 4



TBM	ELEV.	DESCRIPTION
BM 50	892.76	RR SPIKE IN S. SIDE OF PP IN NE QUAD. OF CHERRY ST. AND WATER ST.
CP 806	892.77	PK NAIL NEAR BOW IN NW. QUAD OF CHERRY ST. AND WATER ST.
CP 805	894.30	REBAR W/CAP IN FRONT YARD OF HOUSE #320

NOTE: TREES AND STUMPS WITH X SYMBOL ARE TO BE REMOVED BY CONTRACTOR. TREES WITH O ARE TO BE CUT DOWN BY CITY, WITH STUMPS REMOVED BY CONTRACTOR.



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**PLAN & PROFILE  
CHERRY STREET**

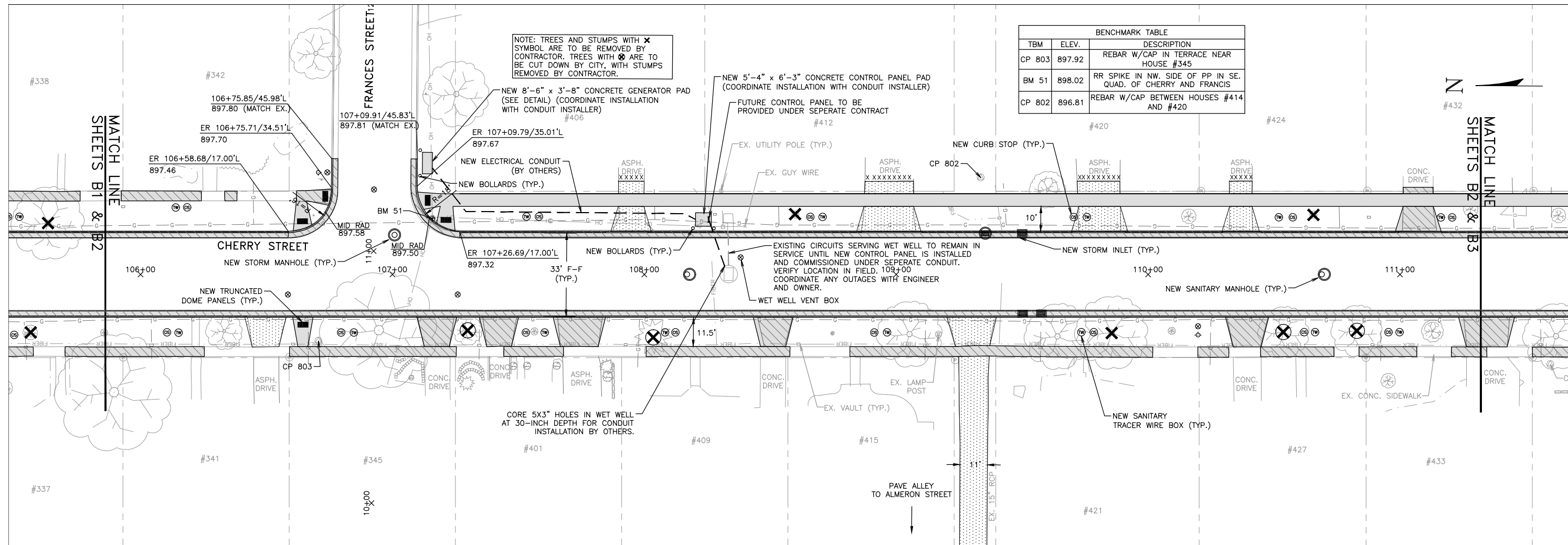
Station 100+00 To Station 106+00

**2024 STREET AND UTILITY IMPROVEMENTS**  
Cherry Street, Mill Street, and Railroad Street  
City of Evansville, Wisconsin

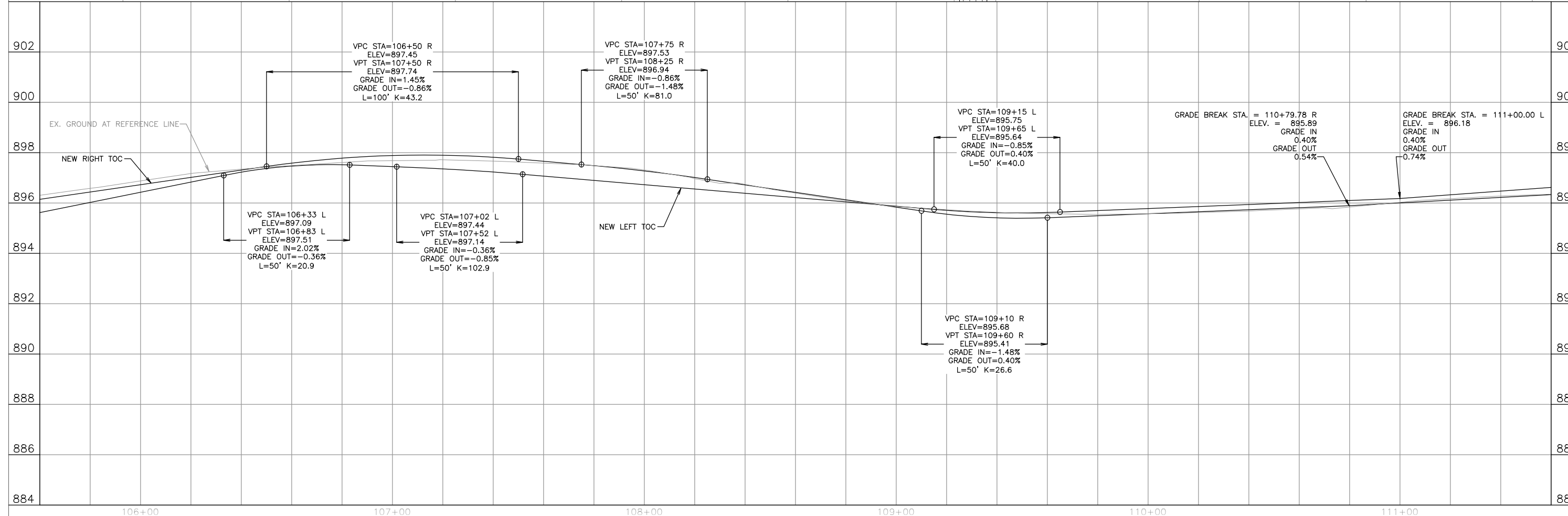
PROJECT NO.:	EV 124
DRAWING FILE:	EV124 CHERRY SHEETS.DWG
DRAWN BY:	A.M.C.
CHECKED BY:	N.R.B.
DATE:	2-14-25
REVISIONS:	

SCALE: HORIZONTAL 1" = 40'  
VERTICAL 1" = 4'

SHEET: **B1**



BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION
CP 803	897.92	REBAR W/CAP IN TERRACE NEAR HOUSE #345
BM 51	898.02	RR SPIKE IN NW. SIDE OF PP IN SE. QUAD. OF CHERRY AND FRANCIS
CP 802	896.81	REBAR W/CAP BETWEEN HOUSES #414 AND #420



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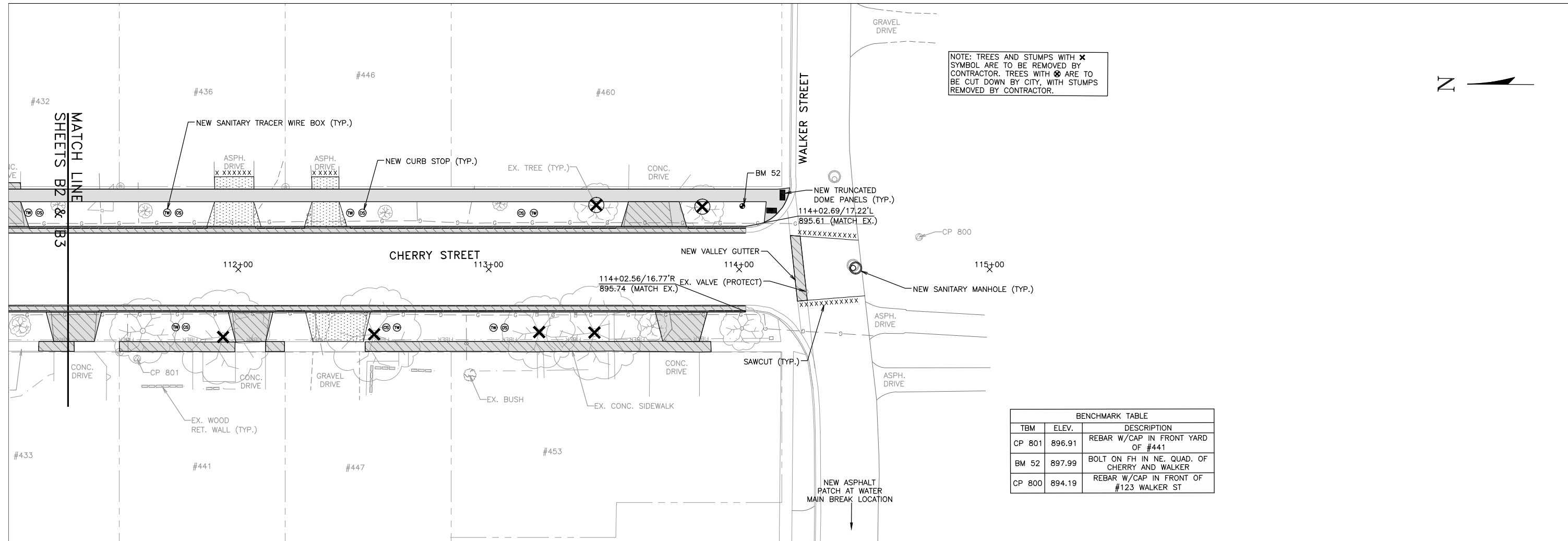
**PLAN & PROFILE**  
CHERRY STREET  
Station 105+60 To Station 111+60

**2024 STREET AND UTILITY IMPROVEMENTS**  
Cherry Street, Mill Street, and Railroad Street  
City of Evansville, Wisconsin

PROJECT NO.: EV 124  
DRAWING FILE: EV124 CHERRY SHEETS.DWG  
DRAWN BY: A.M.C.  
CHECKED BY: N.R.B.  
DATE: 2-14-25  
REVISIONS:

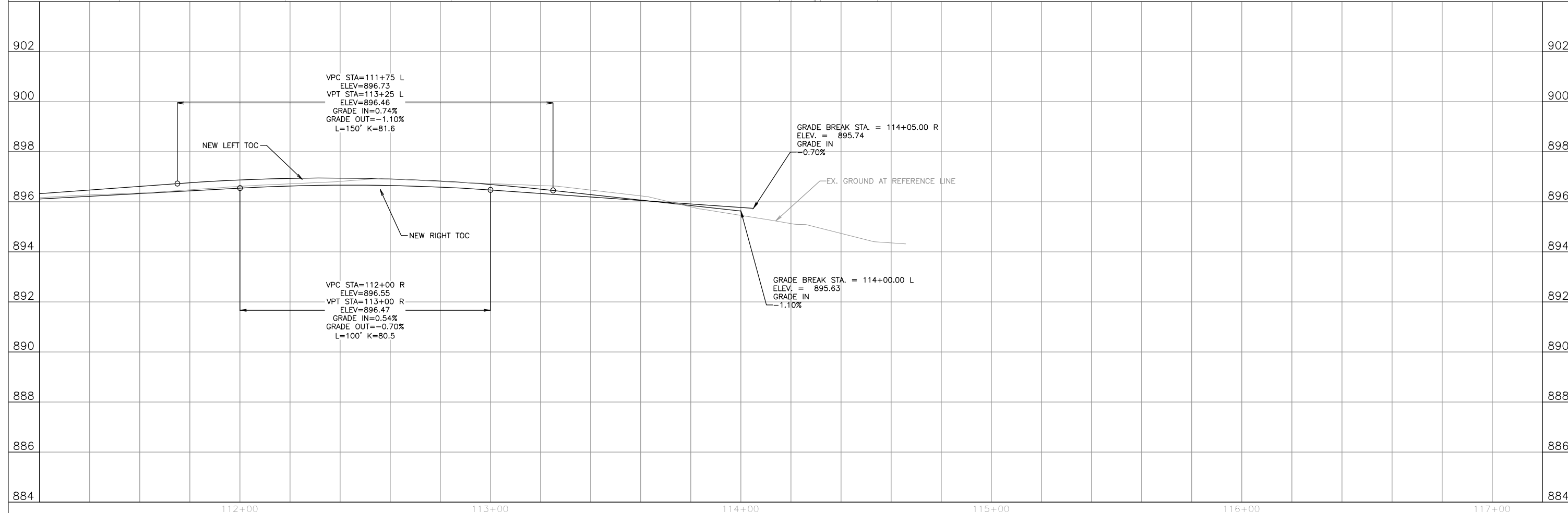
SCALE: HORIZONTAL 1"=20'  
VERTICAL 1"=2'

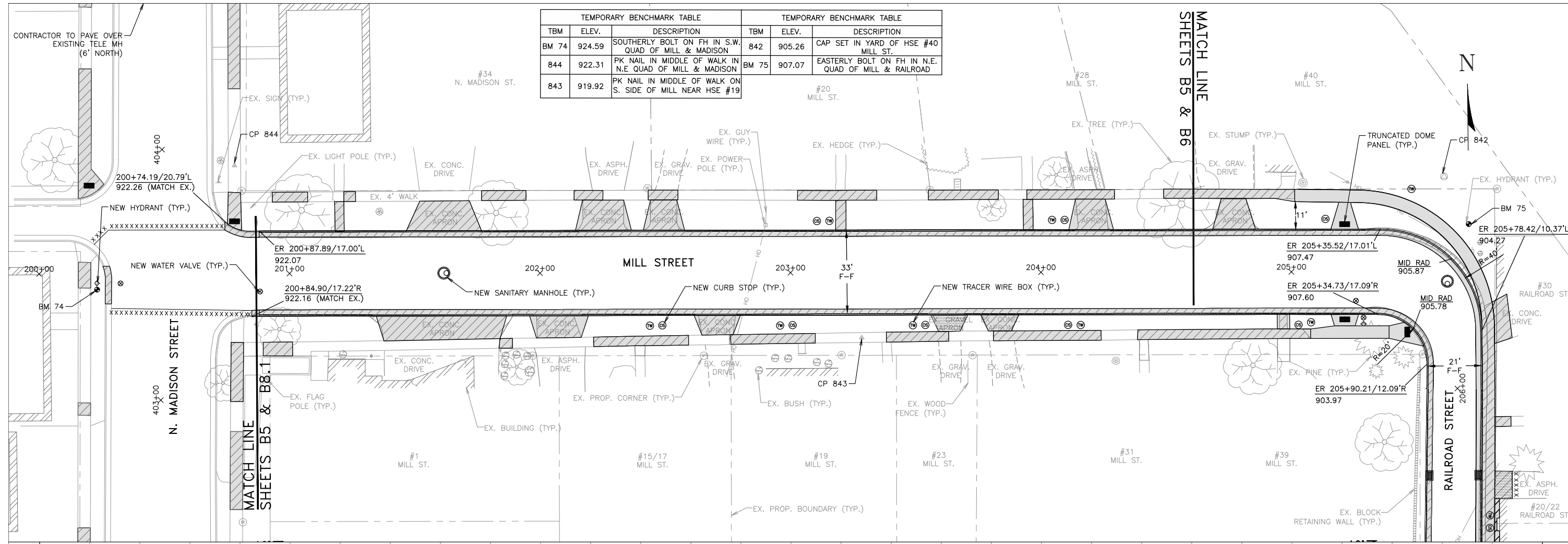
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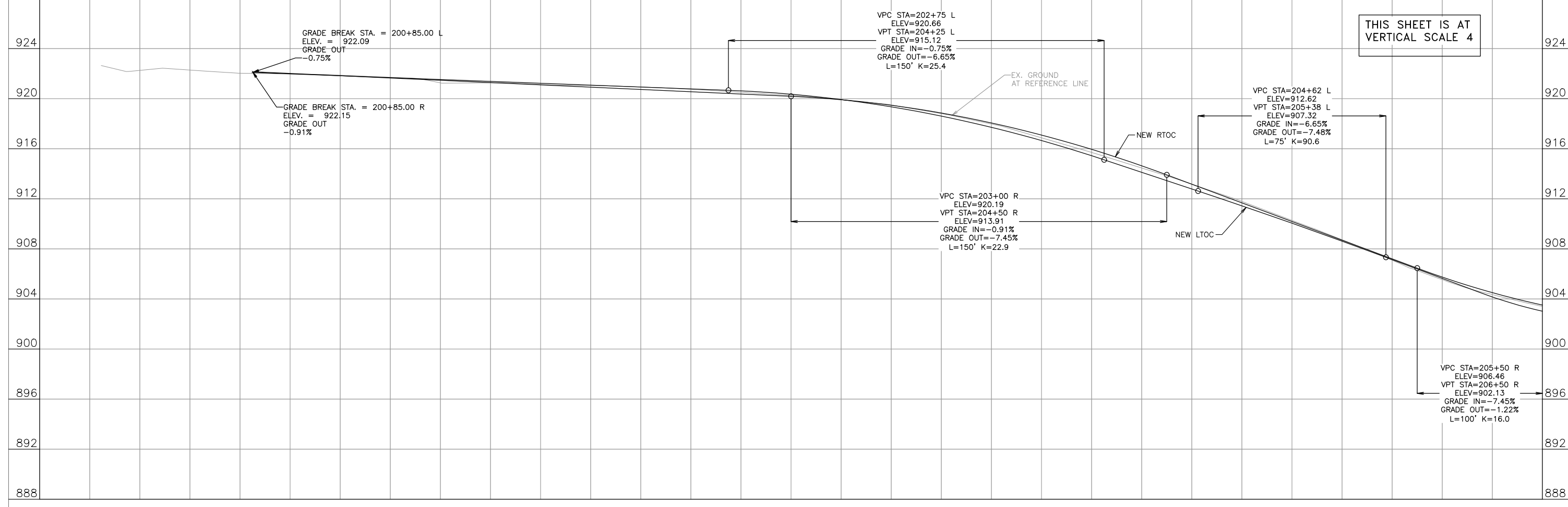
NOTE: TREES AND STUMPS WITH X SYMBOL ARE TO BE REMOVED BY CONTRACTOR. TREES WITH O ARE TO BE CUT DOWN BY CITY, WITH STUMPS REMOVED BY CONTRACTOR.

BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION
CP 801	896.91	REBAR W/CAP IN FRONT YARD OF #441
BM 52	897.99	BOLT ON FH IN NE. QUAD. OF CHERRY AND WALKER
CP 800	894.19	REBAR W/CAP IN FRONT OF #123 WALKER ST





TEMPORARY BENCHMARK TABLE			TEMPORARY BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION	TBM	ELEV.	DESCRIPTION
BM 74	924.59	SOUTHERLY BOLT ON FH IN S.W. QUAD OF MILL & MADISON	842	905.26	CAP SET IN YARD OF HSE #40 MILL ST.
844	922.31	PK NAIL IN MIDDLE OF WALK IN N.E. QUAD OF MILL & MADISON	BM 75	907.07	EASTERLY BOLT ON FH IN N.E. QUAD OF MILL & RAILROAD
843	919.92	PK NAIL IN MIDDLE OF WALK ON S. SIDE OF MILL NEAR HSE #19			

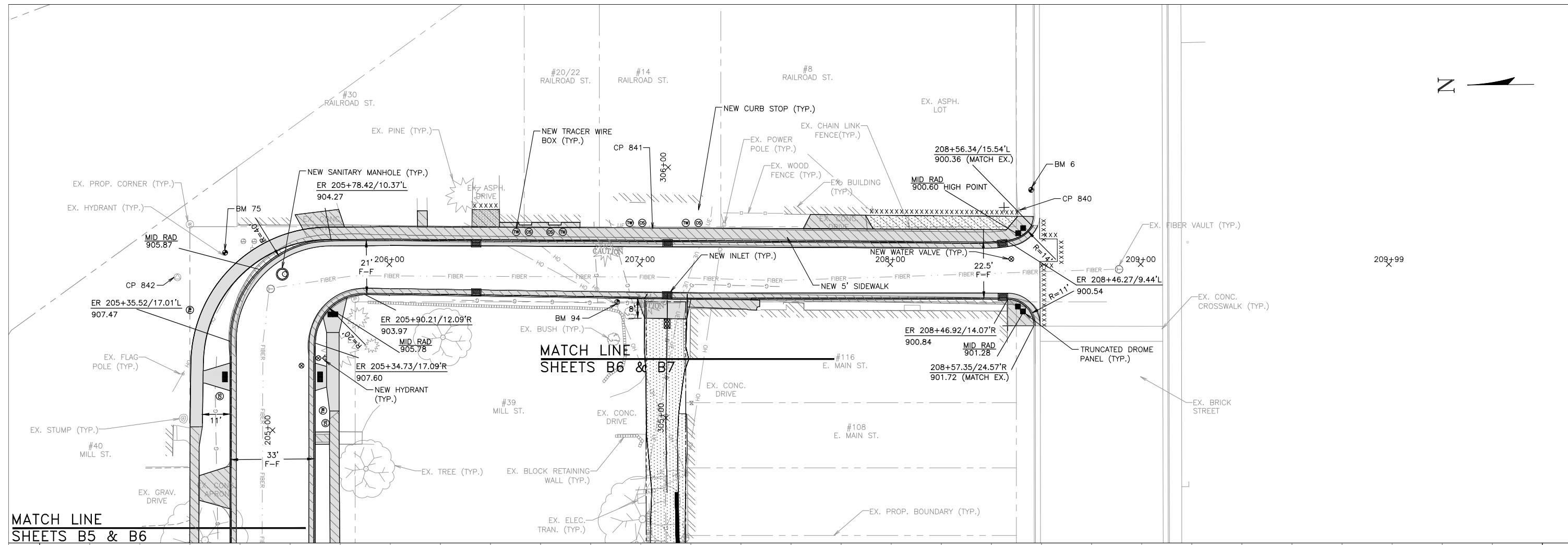


PROJECT NO.: EV 124  
 DRAWING FILE: EV124 MILL-RR SHEETS.DWG  
 DRAWN BY: A.M.C.  
 CHECKED BY: N.R.B.  
 DATE: 2-14-25  
 REVISIONS:  
 SCALE: HORIZONTAL 1"=100'  
 VERTICAL 1"=4'  
 SHEET: B5

2024 STREET AND UTILITY IMPROVEMENTS  
 Cherry St., Mill St., and Railroad St.  
 City of Evansville, Wisconsin

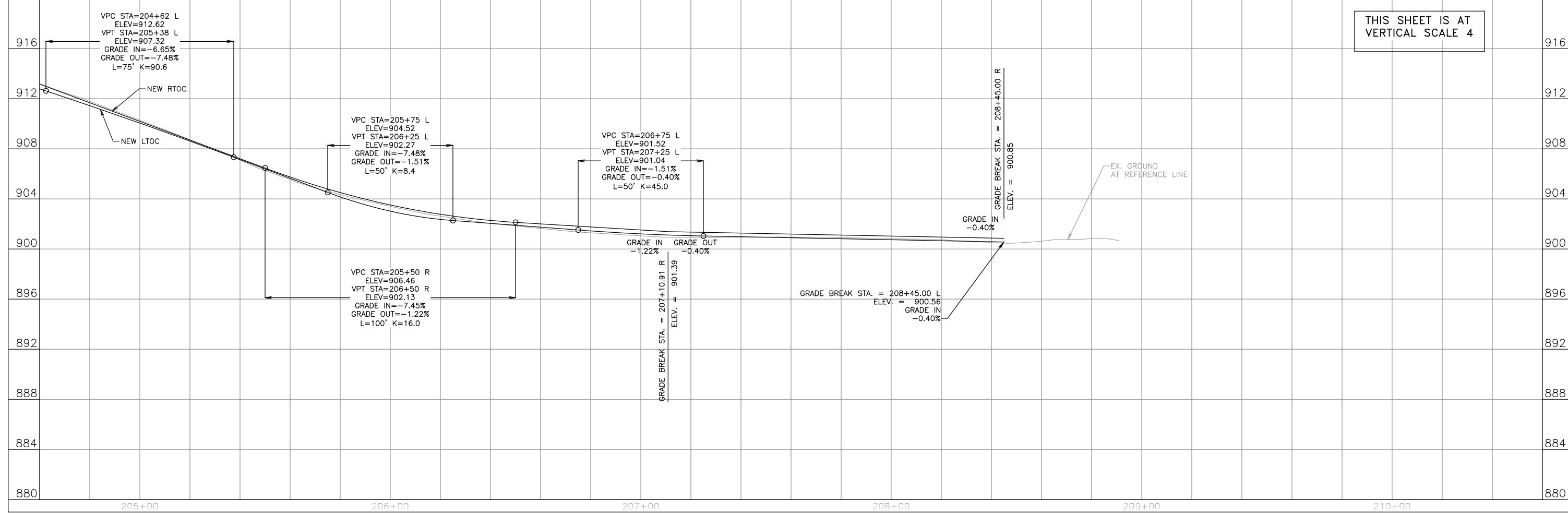
PLAN & PROFILE  
 MILL STREET  
 Station 200+00 To Station 206+00

TOWN & COUNTRY ENGINEERING, INC.  
 6264 Nesbitt Road  
 Madison, WI 53719  
 (608) 273-3350  
 www.tceengineers.net



MATCH LINE  
SHEETS B5 & B6

MATCH LINE  
SHEETS B6 & B7



THIS SHEET IS AT  
VERTICAL SCALE 4

6264 Nesbitt Road  
Madison, WI 53719  
(608) 273-3350  
www.tceengineers.net

**tc** TOWN & COUNTRY ENGINEERING, INC.

PLAN & PROFILE  
RAILROAD STREET  
Station 204+60 To Station 210+60

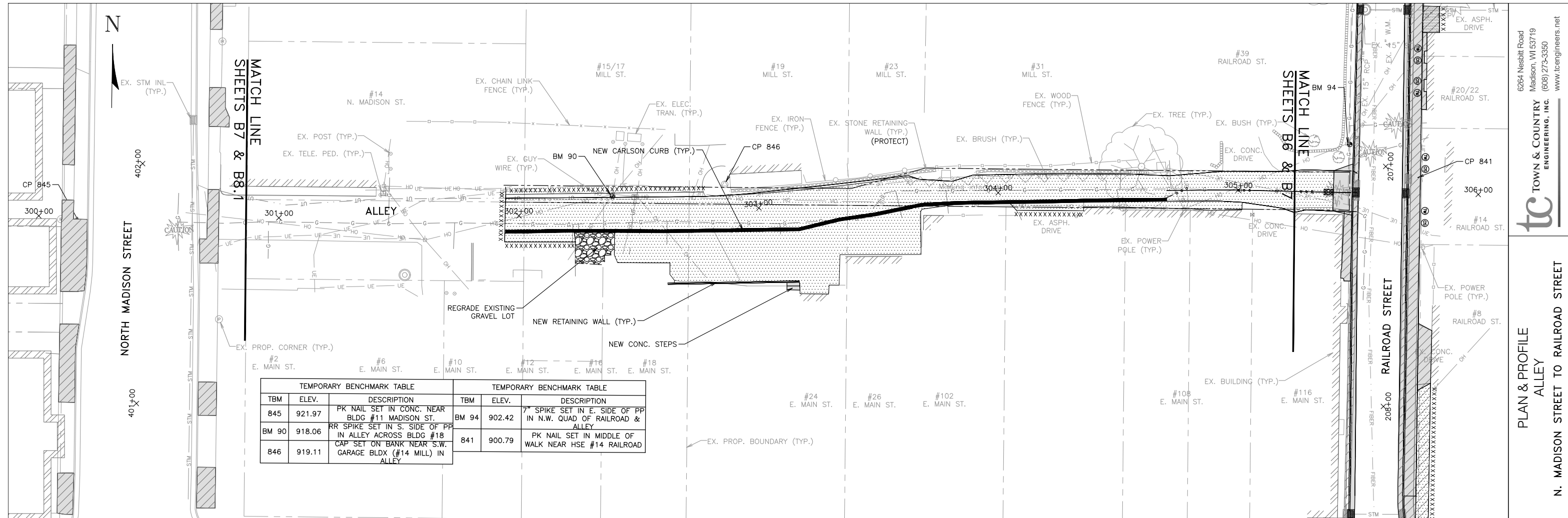
2024 STREET AND UTILITY IMPROVEMENTS  
Cherry St., Mill St., and Railroad St.  
City of Evansville, Wisconsin

PROJECT NO.: EV 124  
DRAWING FILE: EV124-MIL-RR-SHEETS.DWG  
DRAWN BY: A.M.C.  
CHECKED BY: N.R.B.  
DATE: 2-14-25  
REVISIONS:

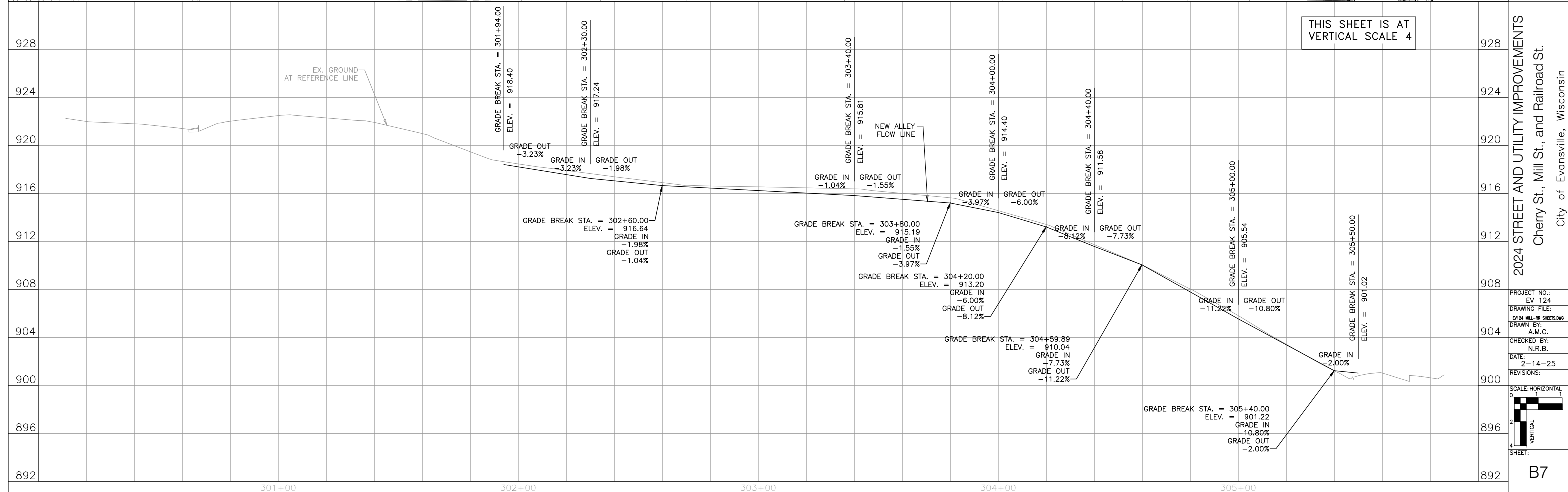
SCALE: HORIZONTAL 1"=20'  
VERTICAL 1"=4'

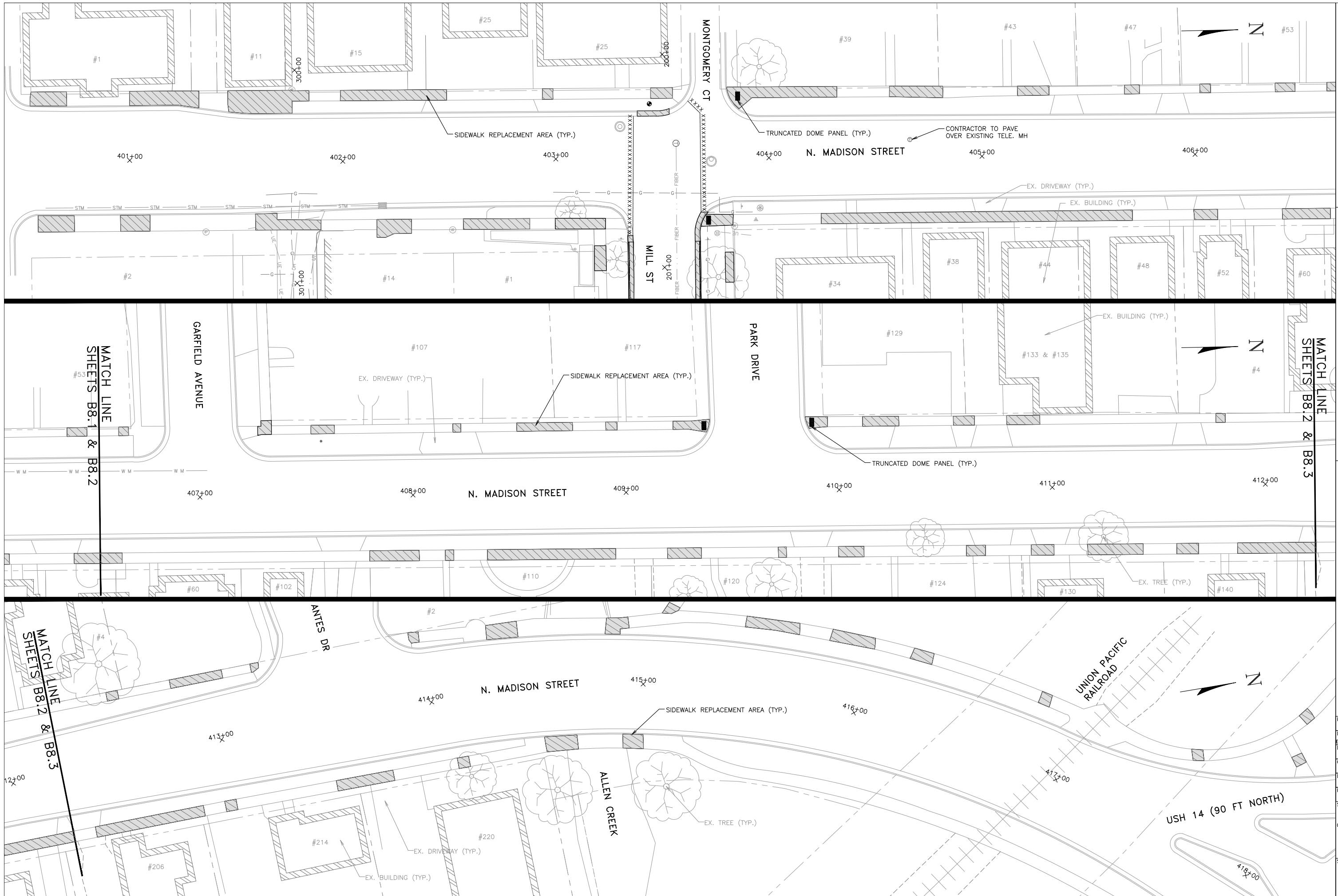
SHEET: B6



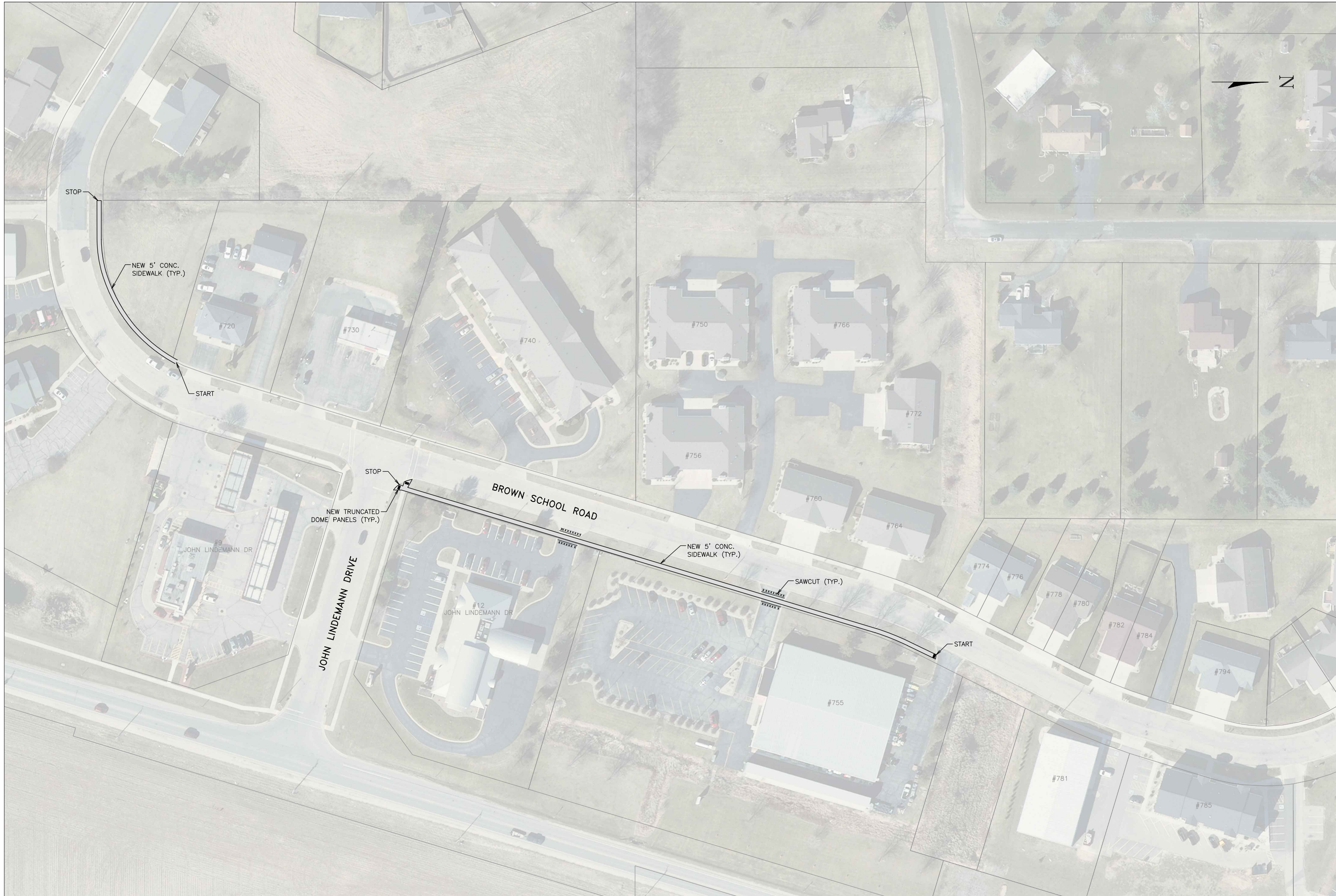


TEMPORARY BENCHMARK TABLE			TEMPORARY BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION	TBM	ELEV.	DESCRIPTION
845	921.97	PK NAIL SET IN CONC. NEAR BLDG #11 MADISON ST.	BM 94	902.42	7" SPIKE SET IN E. SIDE OF PP IN N.W. QUAD OF RAILROAD & ALLEY
BM 90	918.06	RR SPIKE SET IN S. SIDE OF PP IN ALLEY ACROSS BLDG #18	841	900.79	PK NAIL SET IN MIDDLE OF WALK NEAR HSE #14 RAILROAD
846	919.11	CAP SET ON BANK NEAR S.W. GARAGE BLDX (#14 MILL) IN ALLEY			





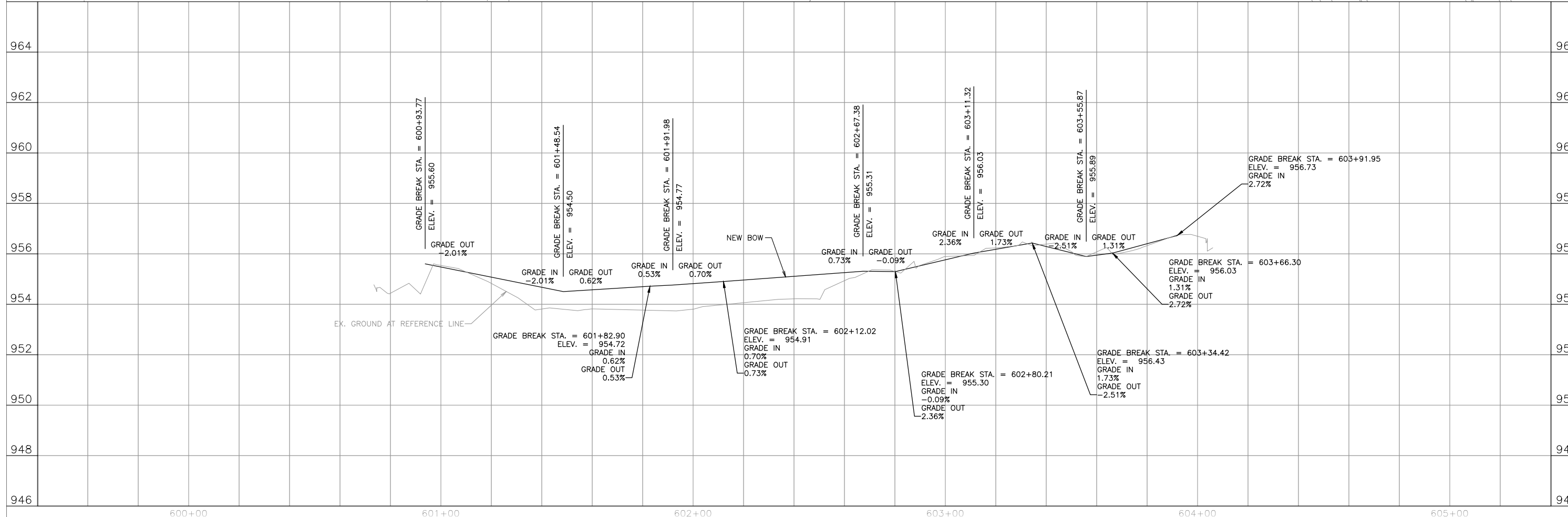
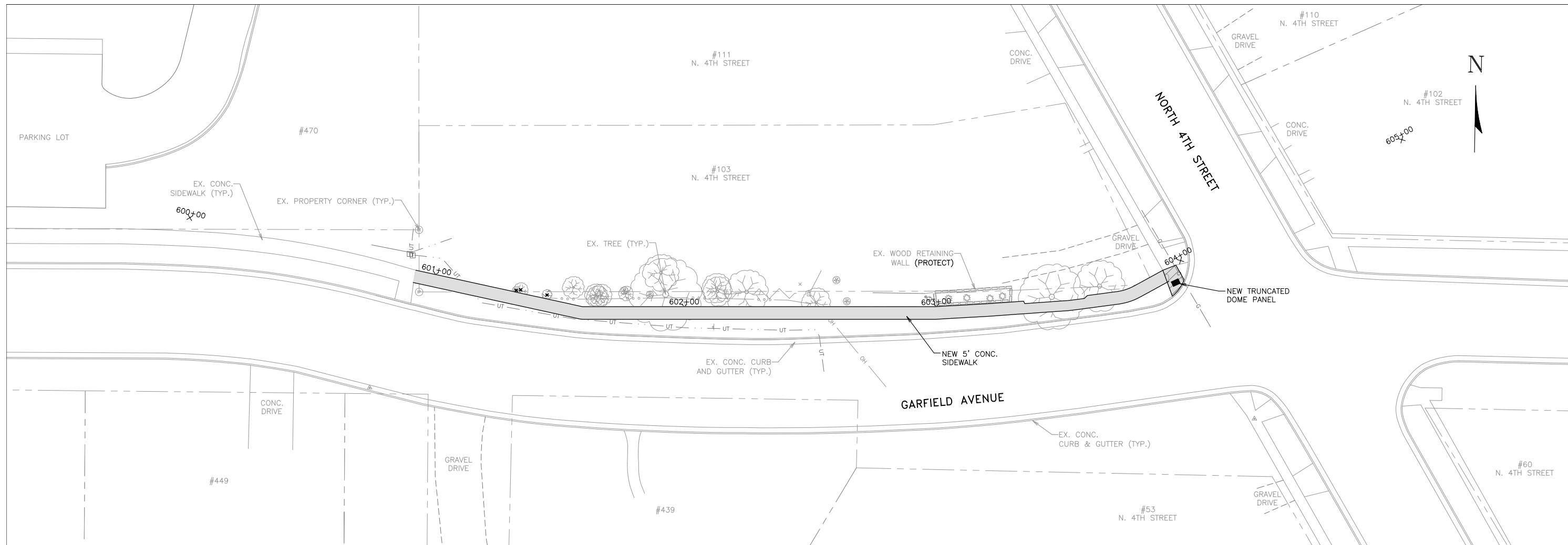


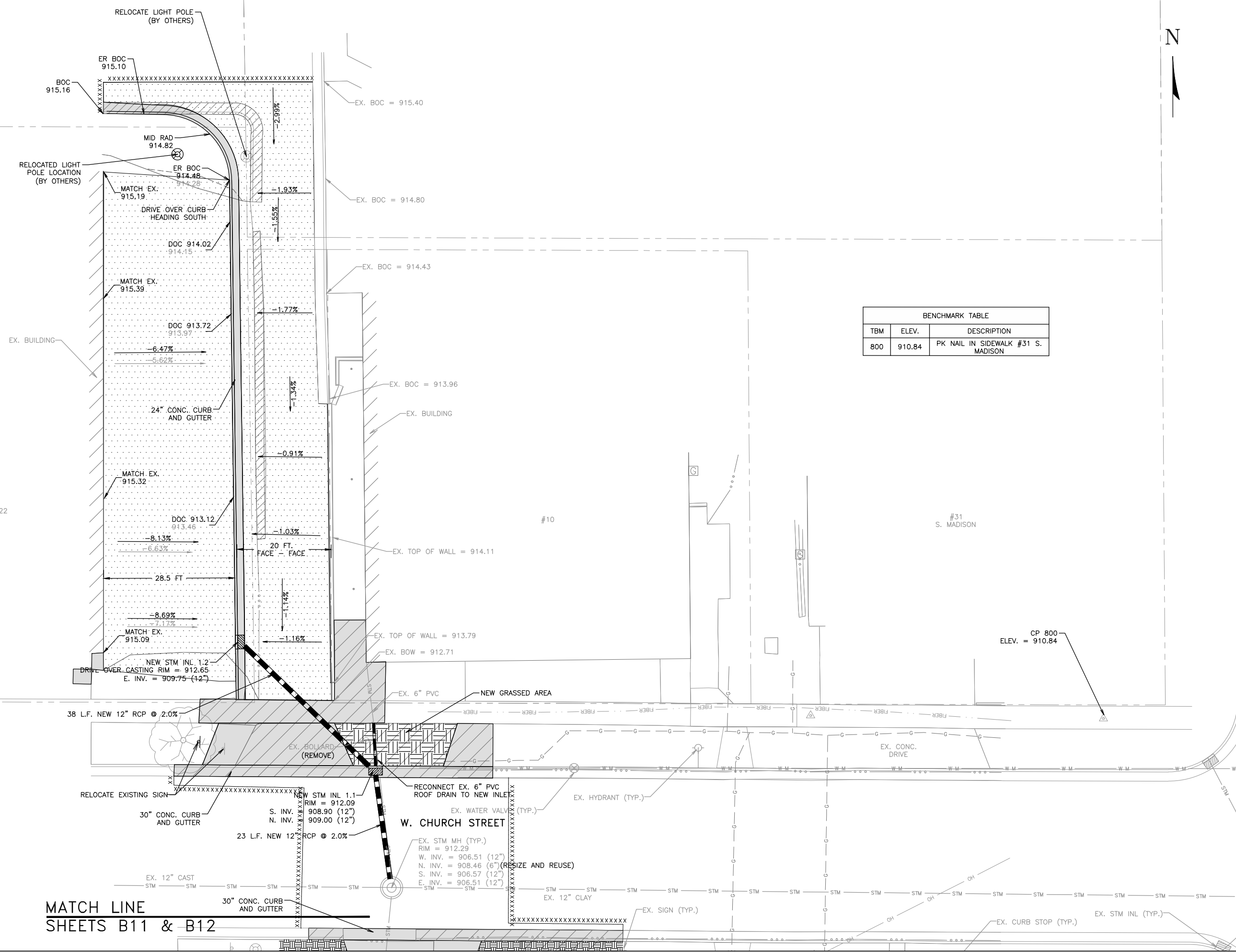


PROJECT NO.:	EV 124
DRAWING FILE:	EV124 CHERRY SHEETS.DWG
DRAWN BY:	A.M.C.
CHECKED BY:	N.R.B.
DATE:	2-14-25
REVISIONS:	
SCALE:	
SHEET:	B9



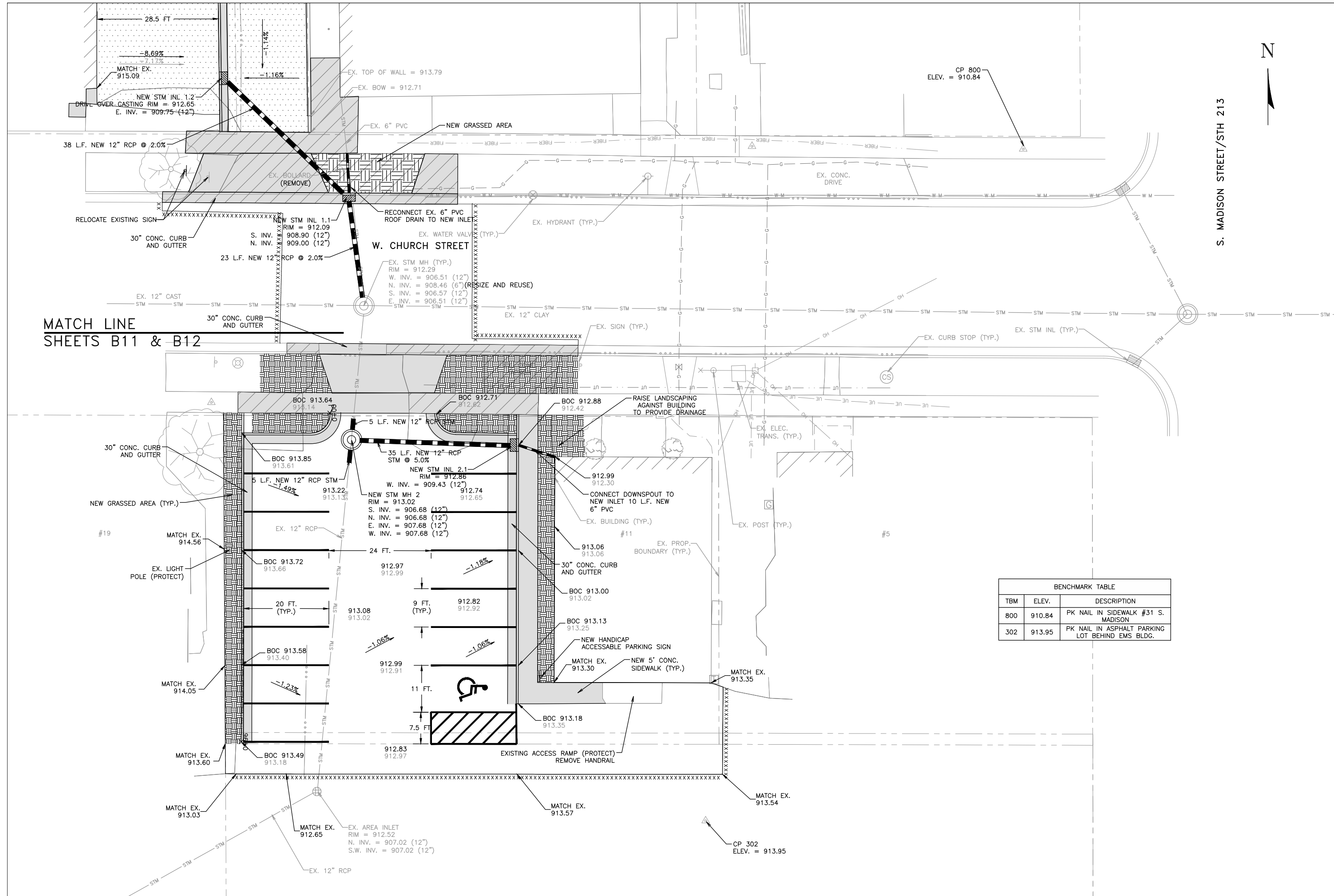






BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION
800	910.84	PK NAIL IN SIDEWALK #31 S. MADISON

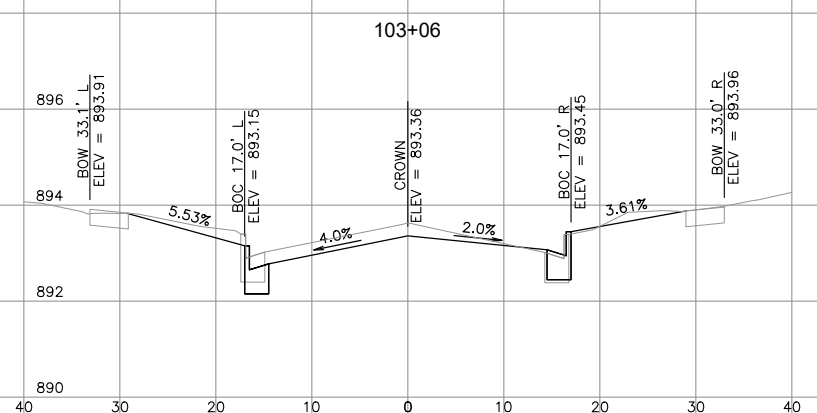
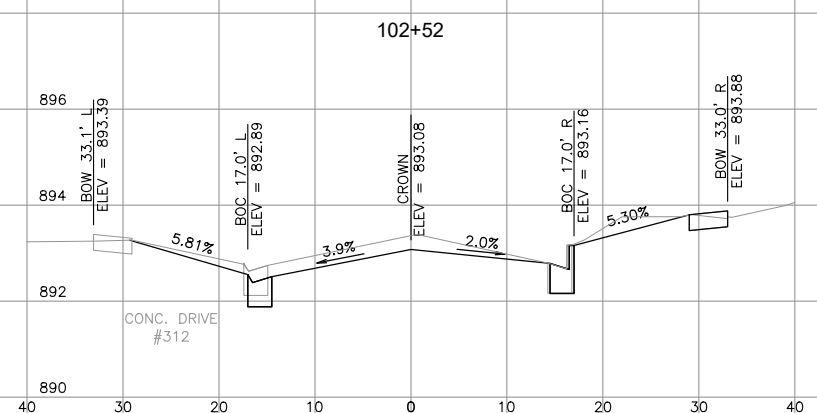
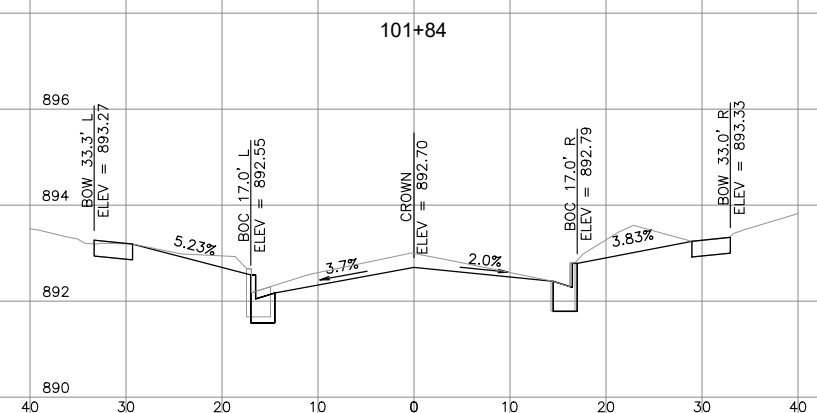
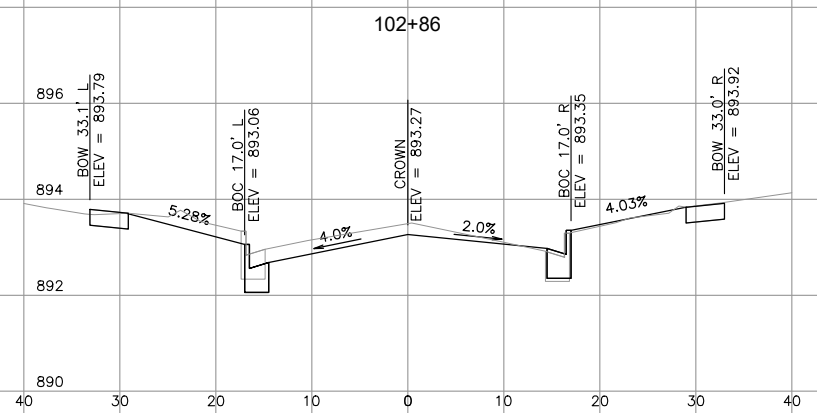
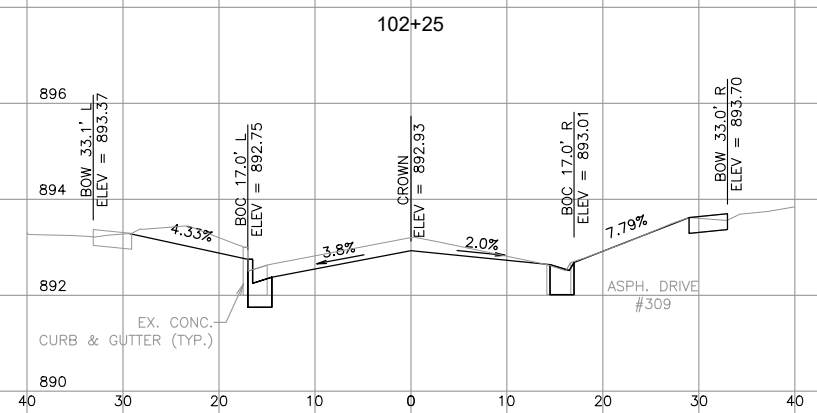
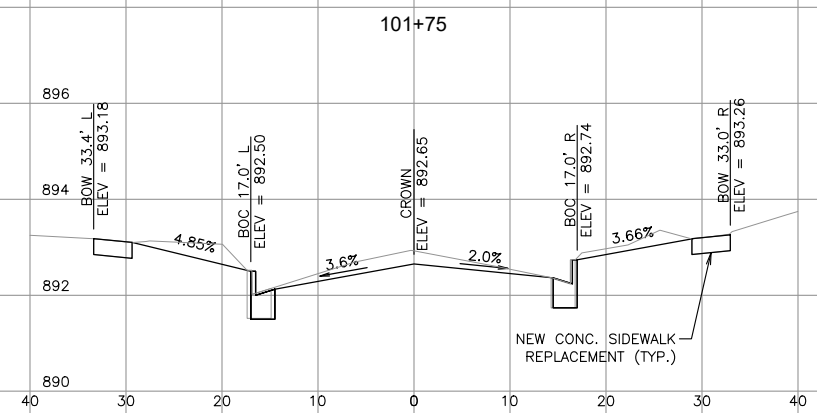
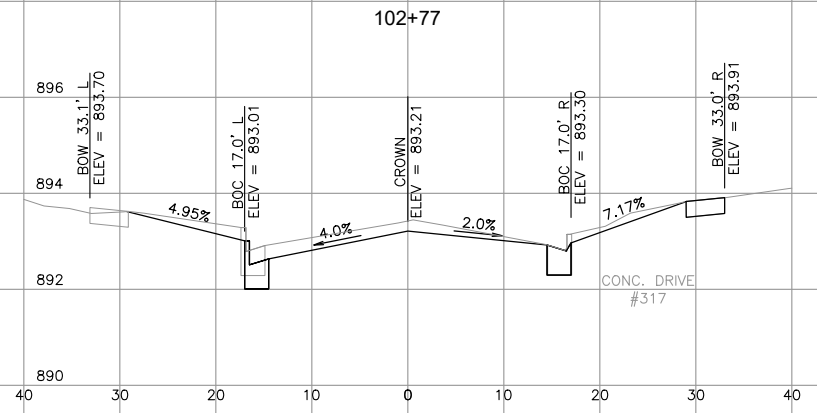
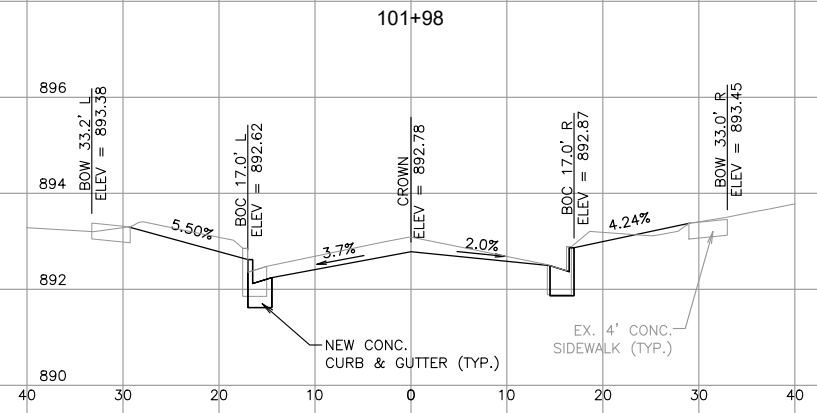
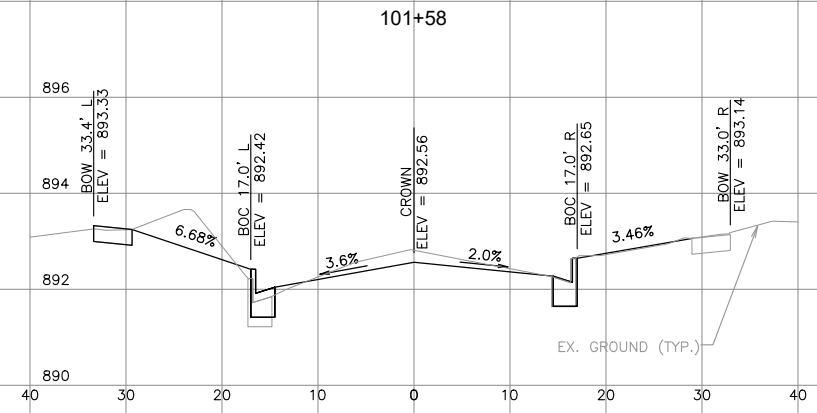
MATCH LINE  
SHEETS B11 & B12



MATCH LINE  
SHEETS B11 & B12

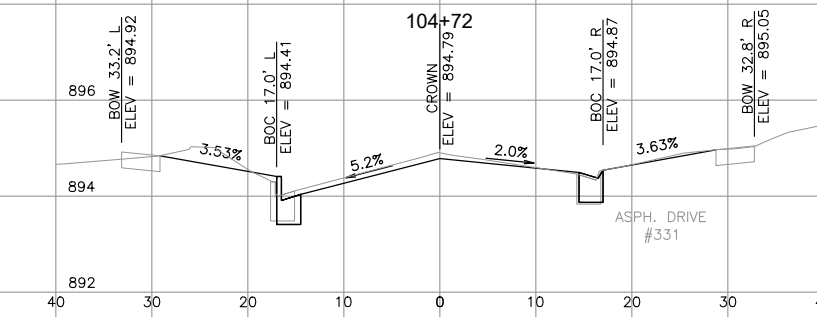
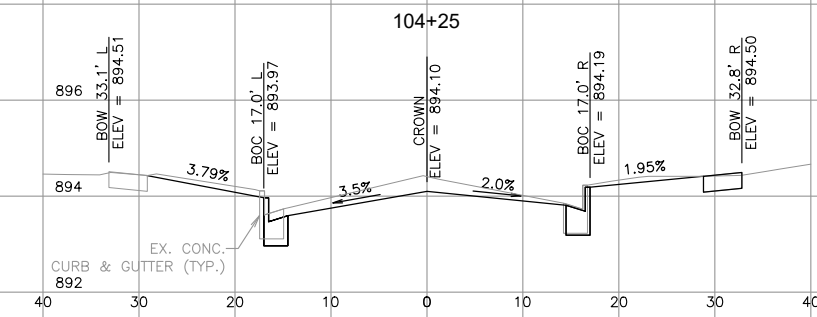
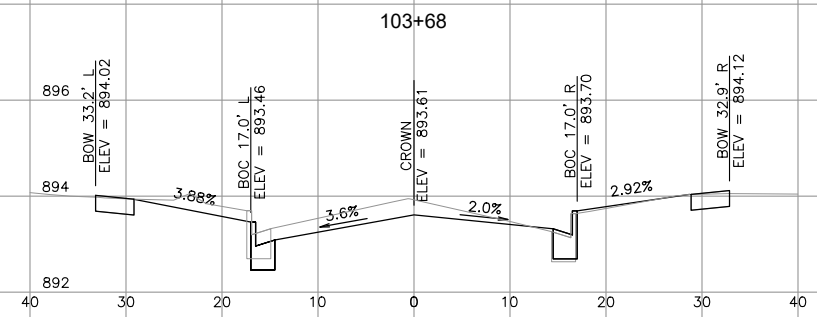
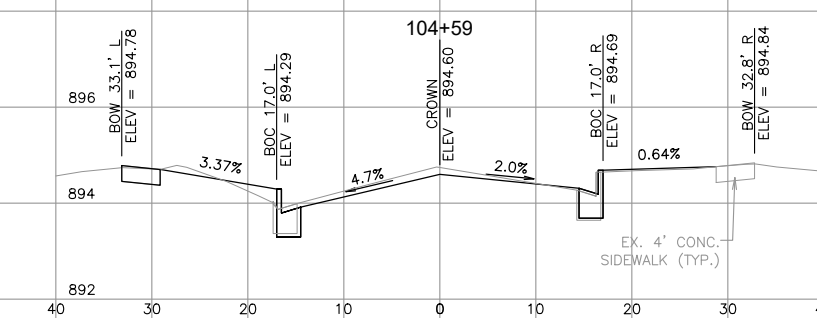
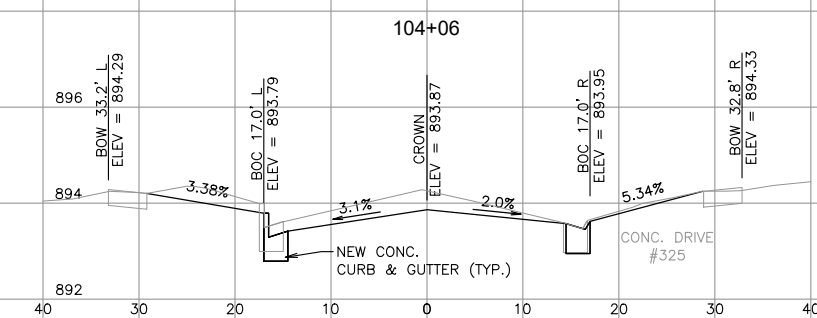
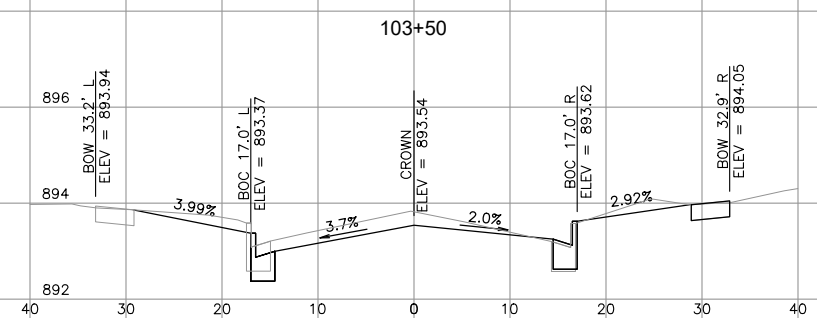
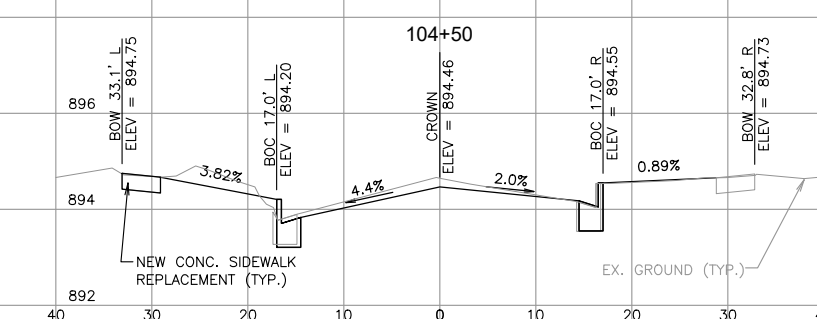
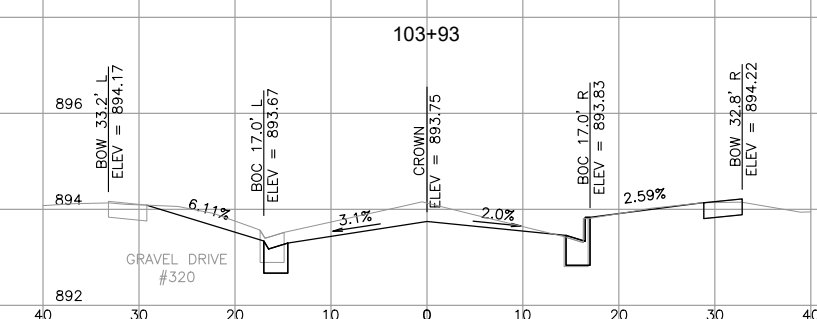
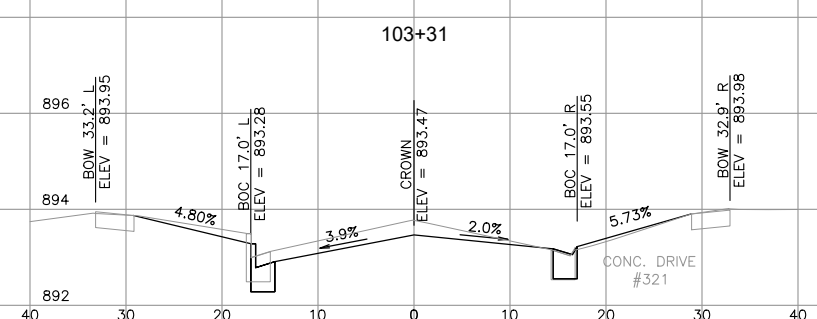
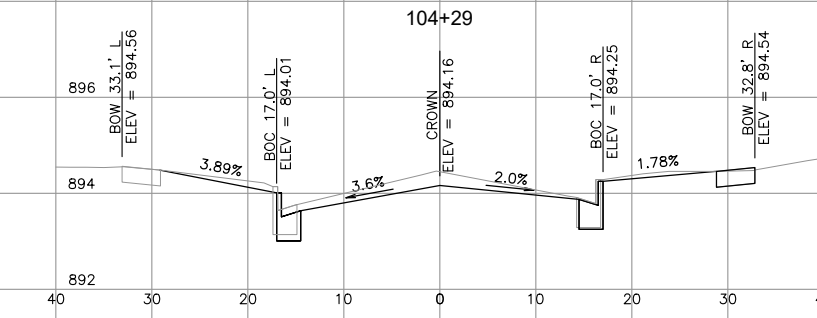
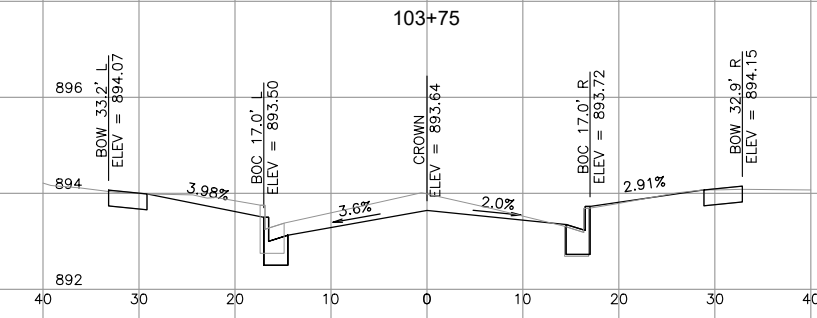
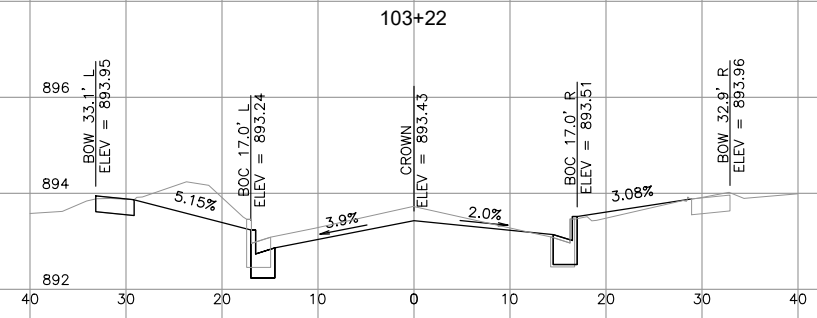
BENCHMARK TABLE		
TBM	ELEV.	DESCRIPTION
800	910.84	PK NAIL IN SIDEWALK #31 S. MADISON
302	913.95	PK NAIL IN ASPHALT PARKING LOT BEHIND EMS BLDG.

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



PROJECT NO.:	EV 124
DRAWING FILE:	EV124_CORRIDOR.DWG
DRAWN BY:	A.M.C.
CHECKED BY:	N.R.B.
DATE:	2-14-25
REVISIONS:	
SCALE: HORIZONTAL	0 2.5 5 10
SCALE: VERTICAL	1 2
SHEET:	X1

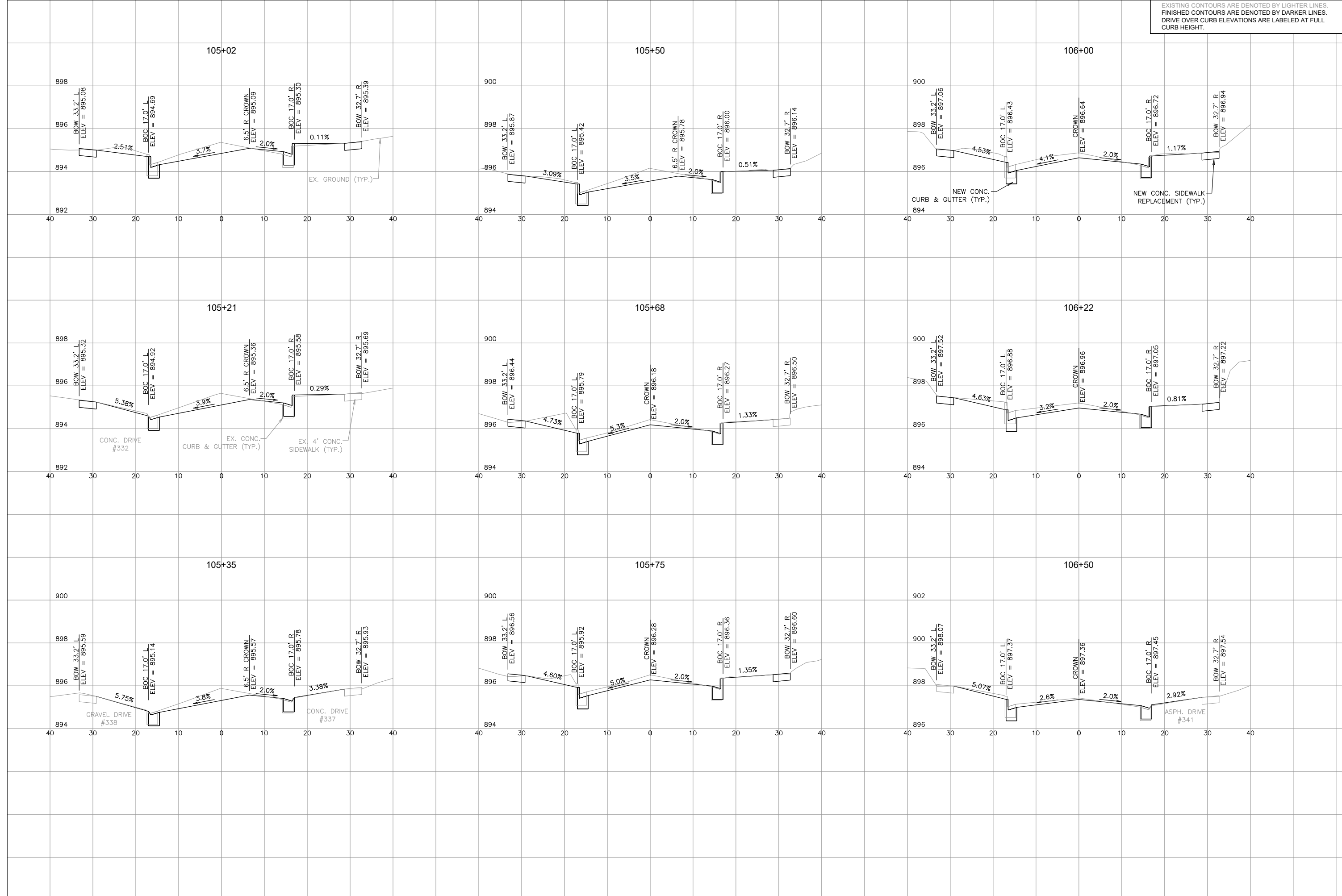
EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



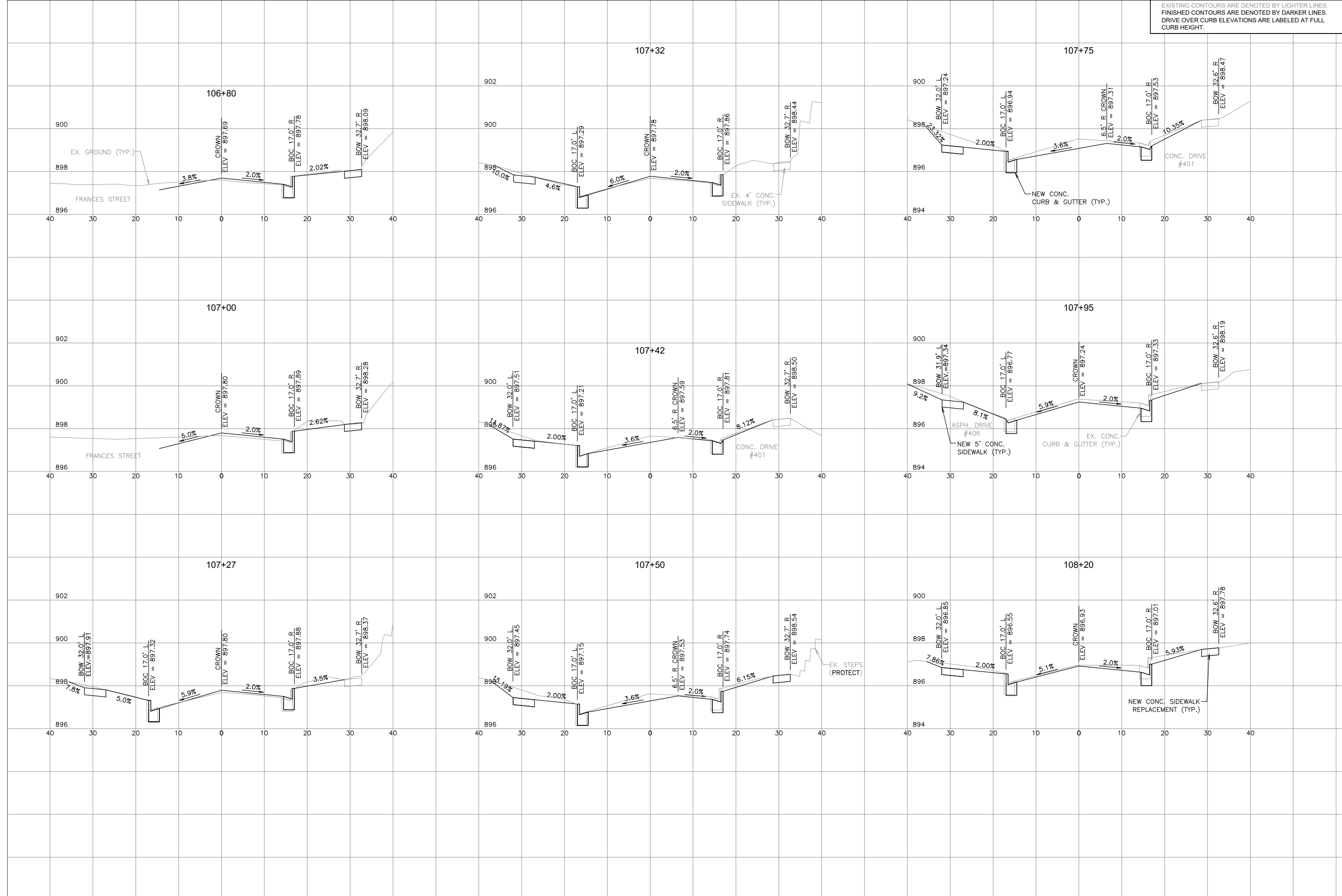
PROJECT NO.:	EV 124
DRAWING FILE:	EV124 CORRIDOR.DWG
DRAWN BY:	A.M.C.
CHECKED BY:	N.R.B.
DATE:	2-14-25
REVISIONS:	
SCALE: HORIZONTAL	0" = 25'
SCALE: VERTICAL	1" = 5'
SHEET:	



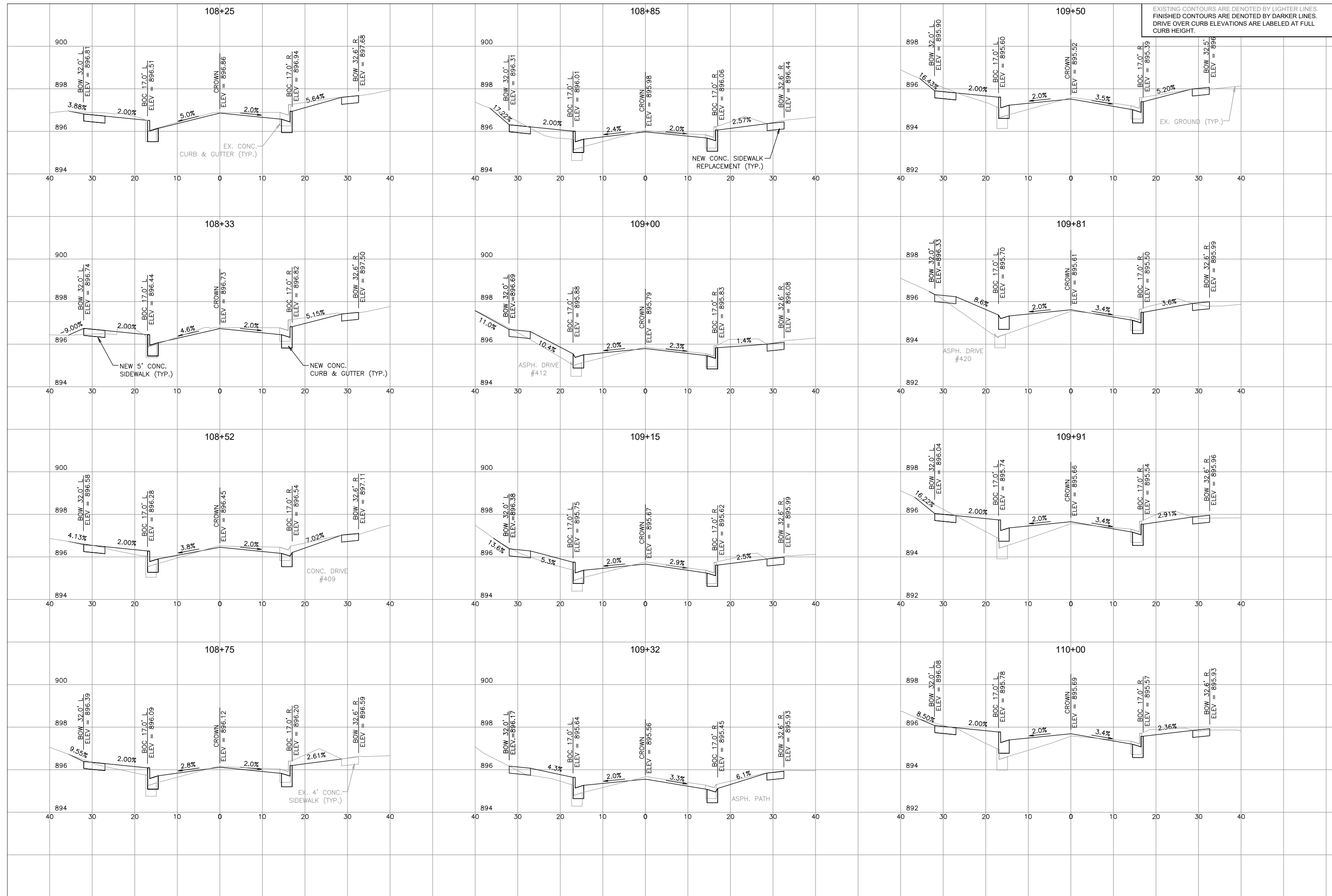
EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.

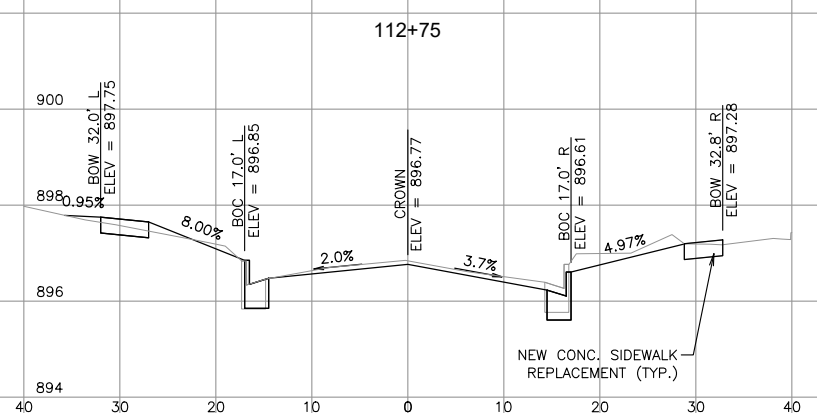
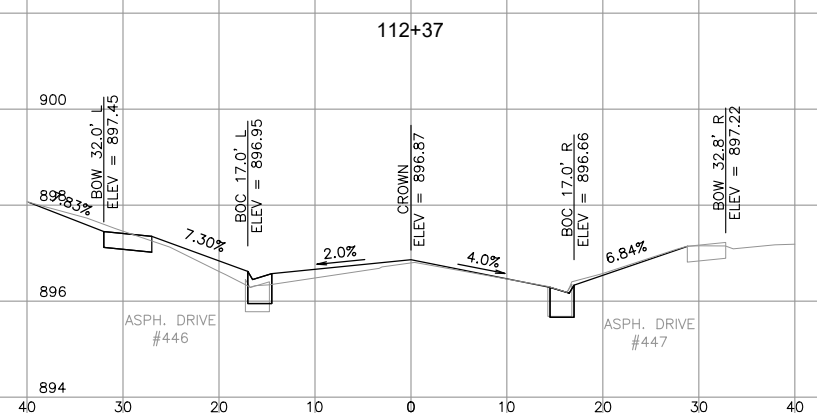
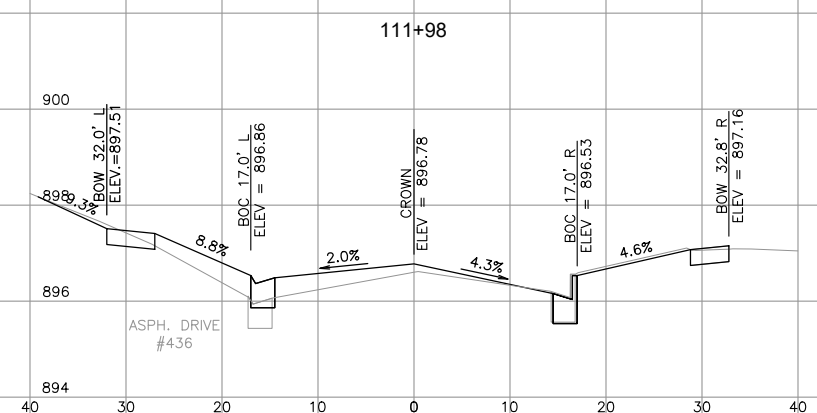
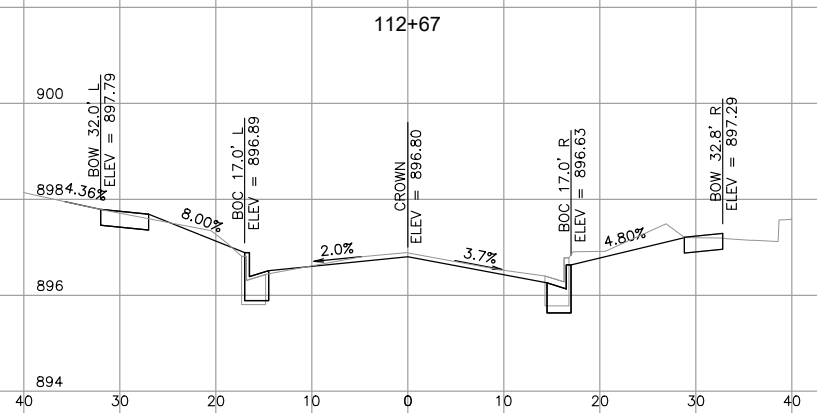
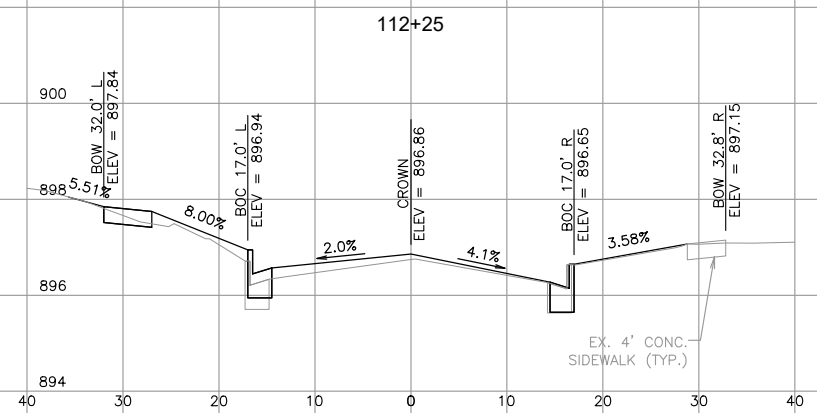
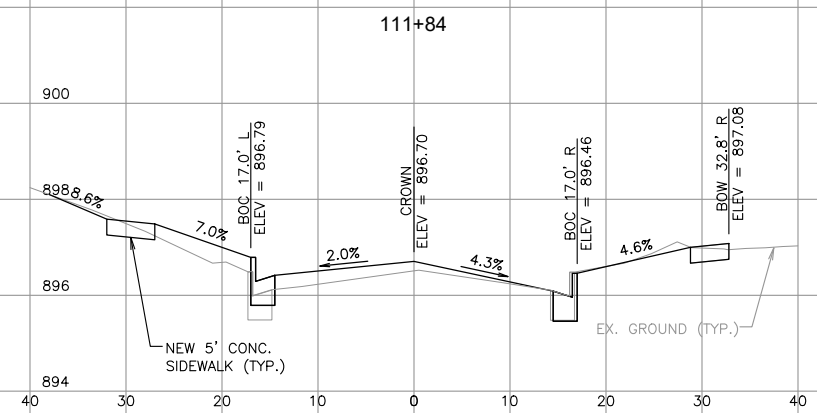
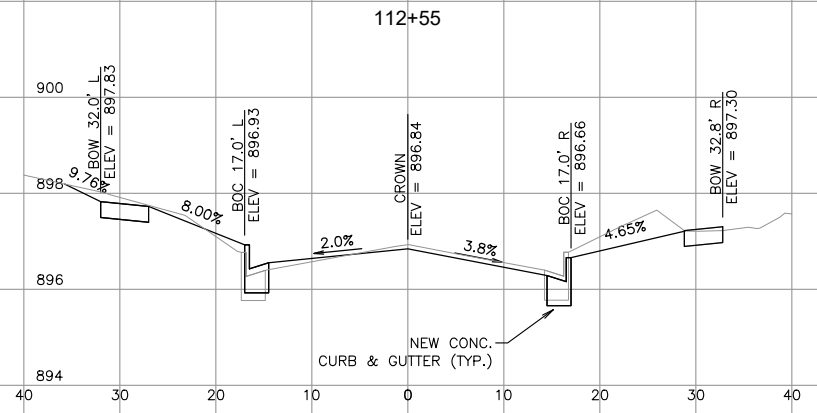
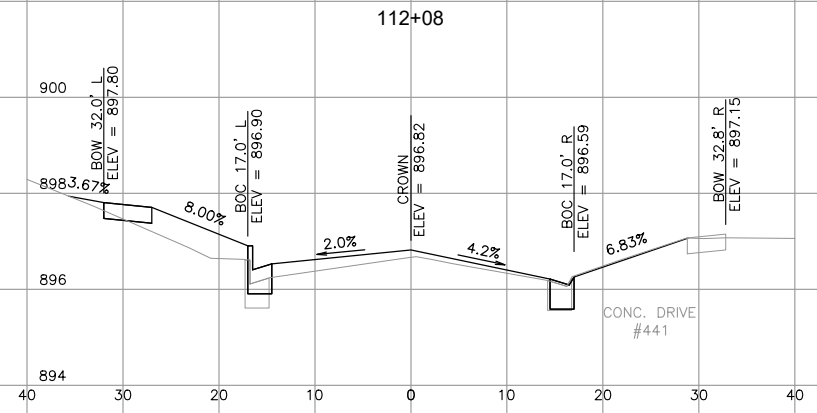
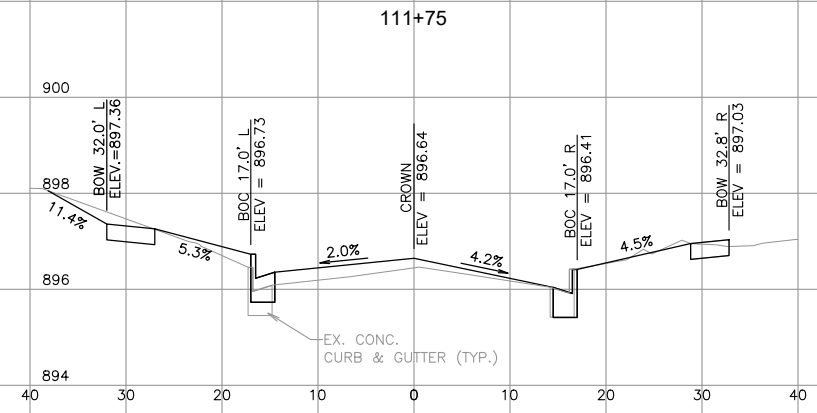


PROJECT NO.:	EV 124
DRAWING FILE:	EV124 CORRIDOR.DWG
DRAWN BY:	A.M.C.
CHECKED BY:	N.R.B.
DATE:	2-14-25
REVISIONS:	
SCALE: HORIZONTAL	0 2.5 5 10
SCALE: VERTICAL	1 2
SHEET:	





EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



PROJECT NO.:  
 EV 124

DRAWING FILE:  
 EV124\_CORRIDOR.DWG

DRAWN BY:  
 A.M.C.

CHECKED BY:  
 N.R.B.

DATE:  
 2-14-25

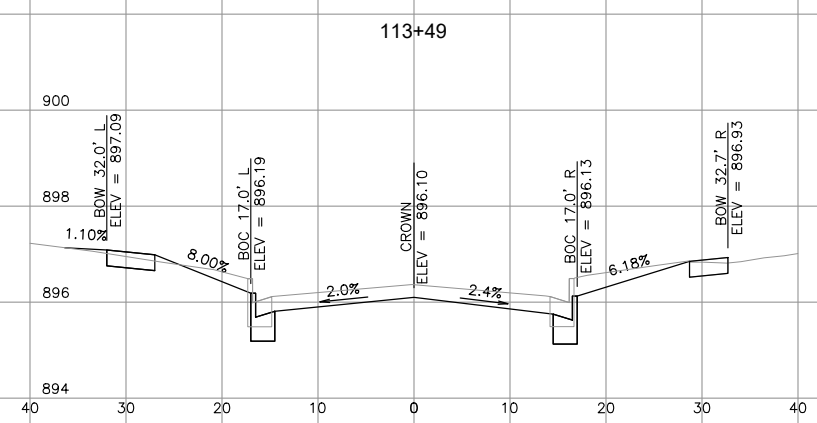
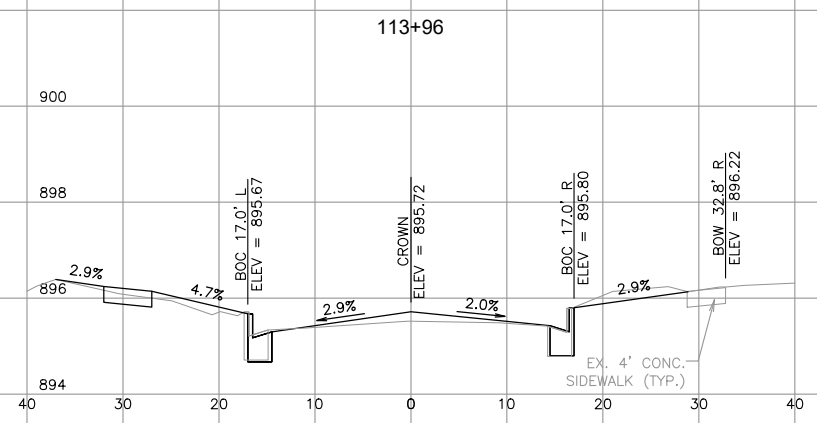
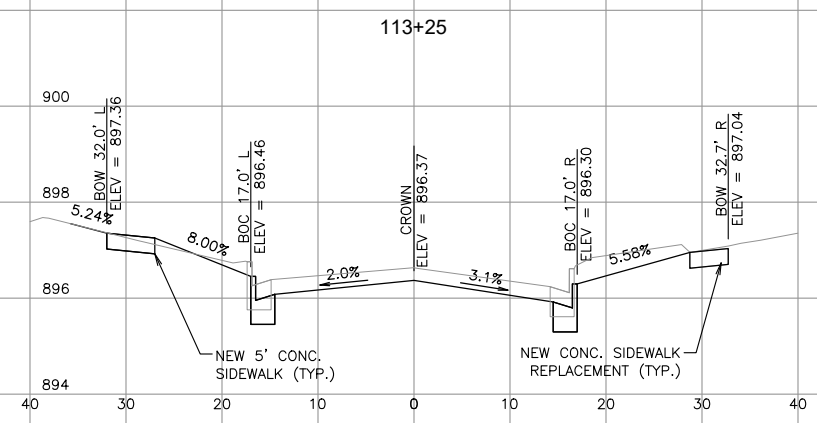
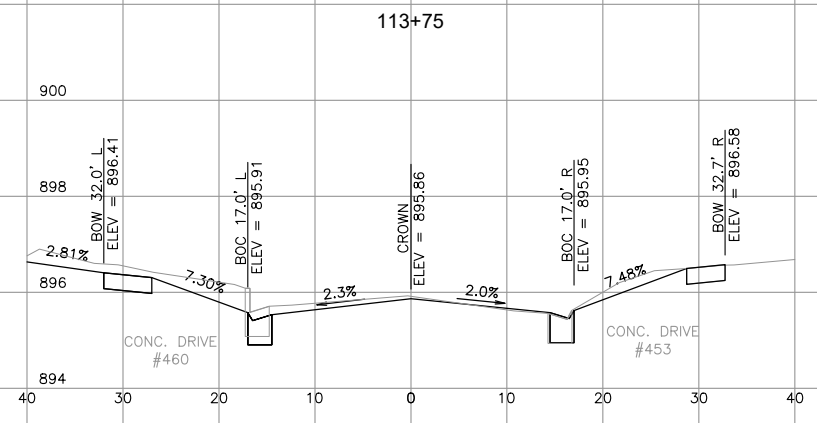
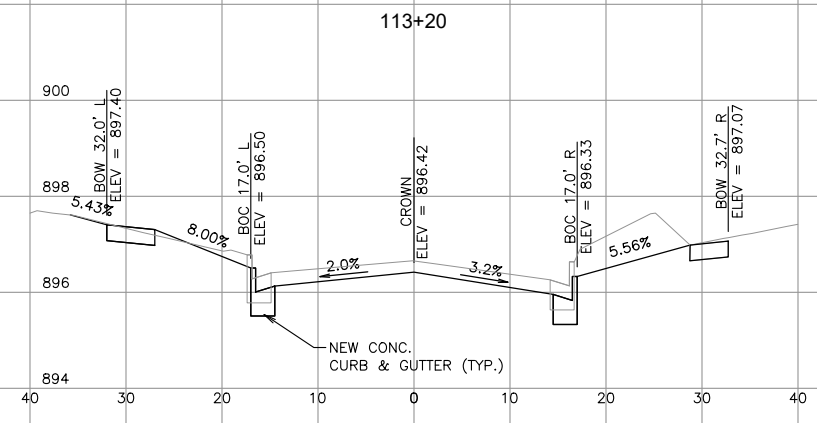
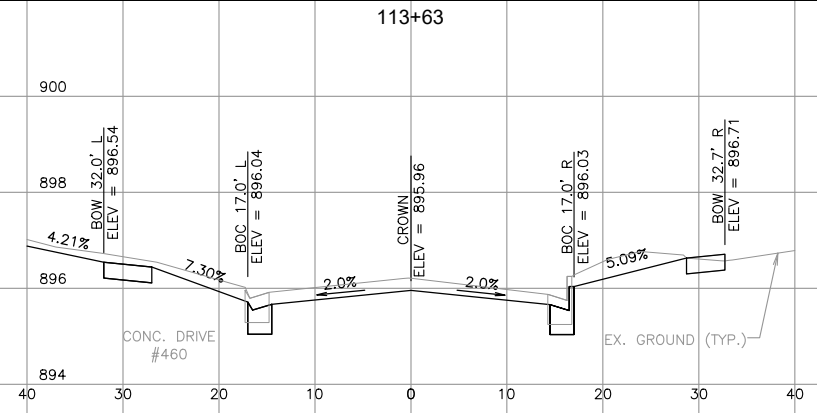
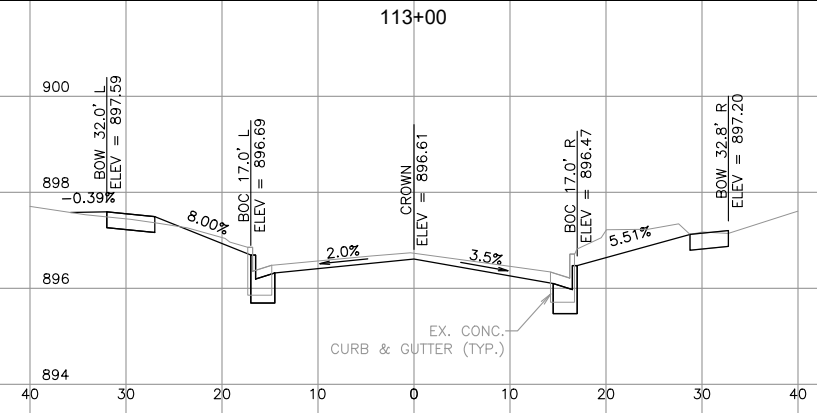
REVISIONS:

SCALE: HORIZONTAL  
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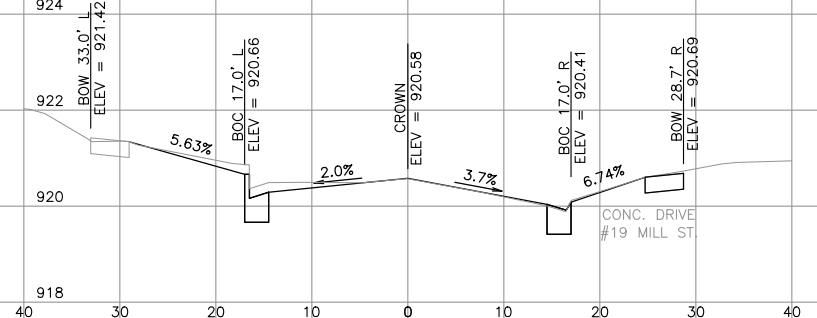
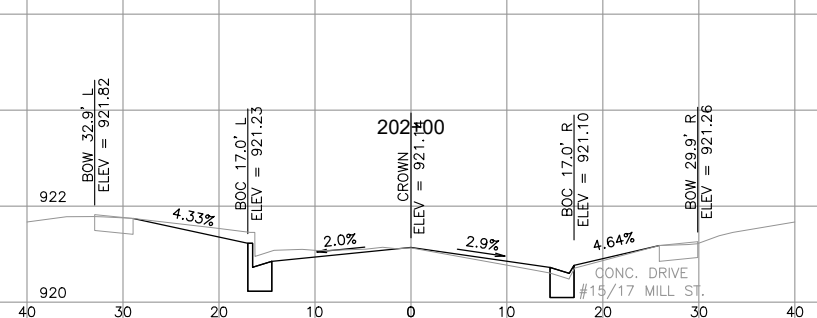
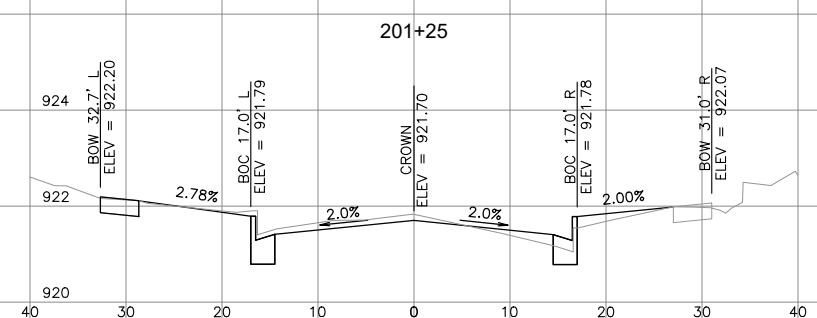
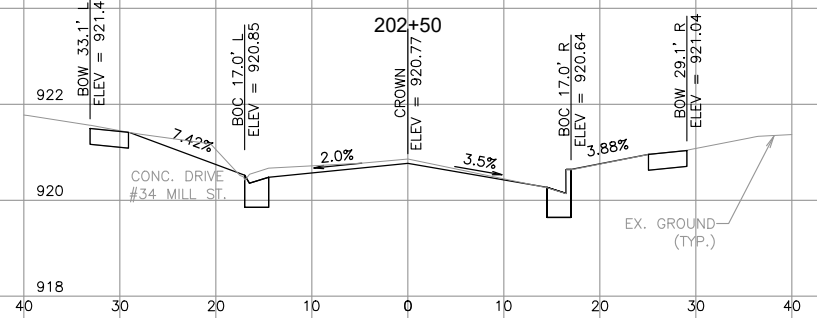
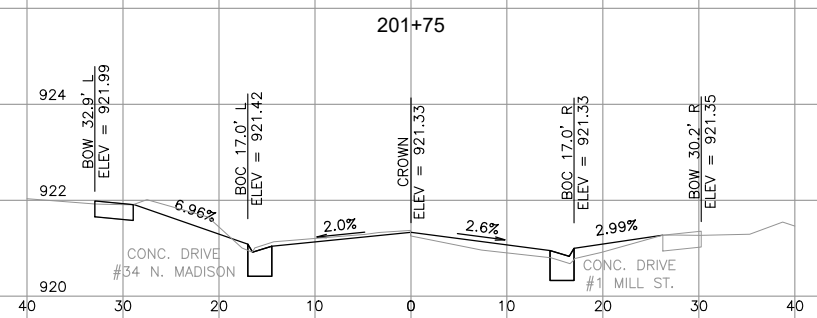
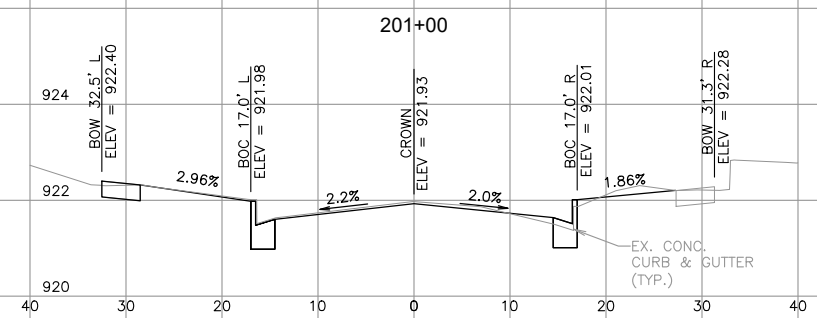
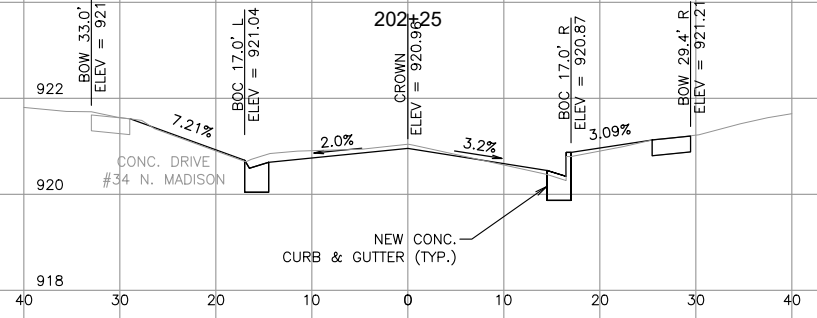
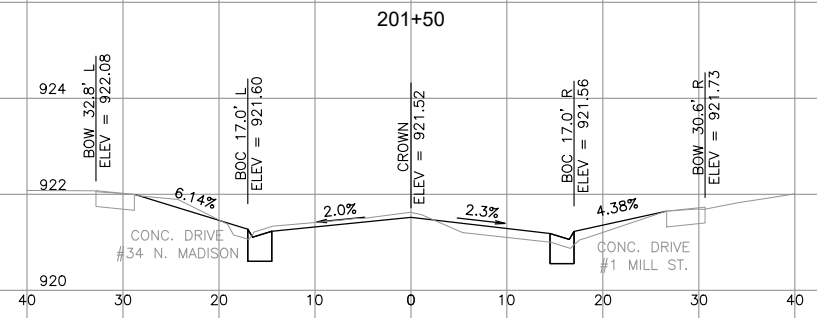
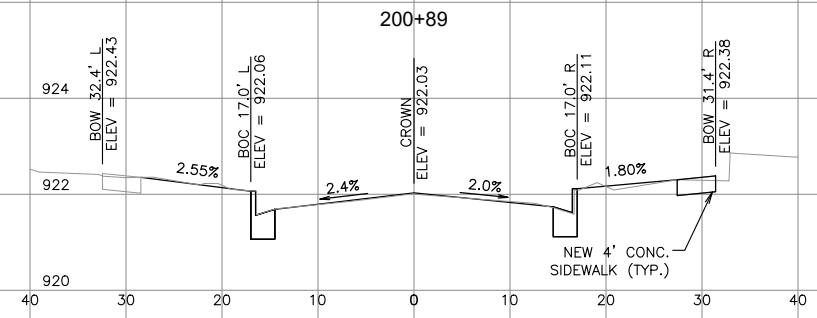
VERTICAL  
 1 2

SHEET:

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



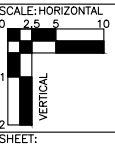
6264 Nesbitt Road  
 Madison, WI 53719  
 (608) 273-3350  
 www.tceengineers.net



CROSS SECTIONS  
 MILL STREET  
 Station 200+89 To Station 202+75

2025 STREET AND UTILITY IMPROVEMENTS  
 Cherry St., Mill St., and Railroad St.  
 City of Evansville, Wisconsin

PROJECT NO.:  
 EV 124  
 DRAWING FILE:  
 EV124 CORRIDOR.DWG  
 DRAWN BY:  
 A.M.C.  
 CHECKED BY:  
 N.R.B.  
 DATE:  
 2-14-25  
 REVISIONS:



X9

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.

6264 Nesbitt Road  
 Madison, WI 53719  
 (608) 273-3350  
 www.tceengineers.net

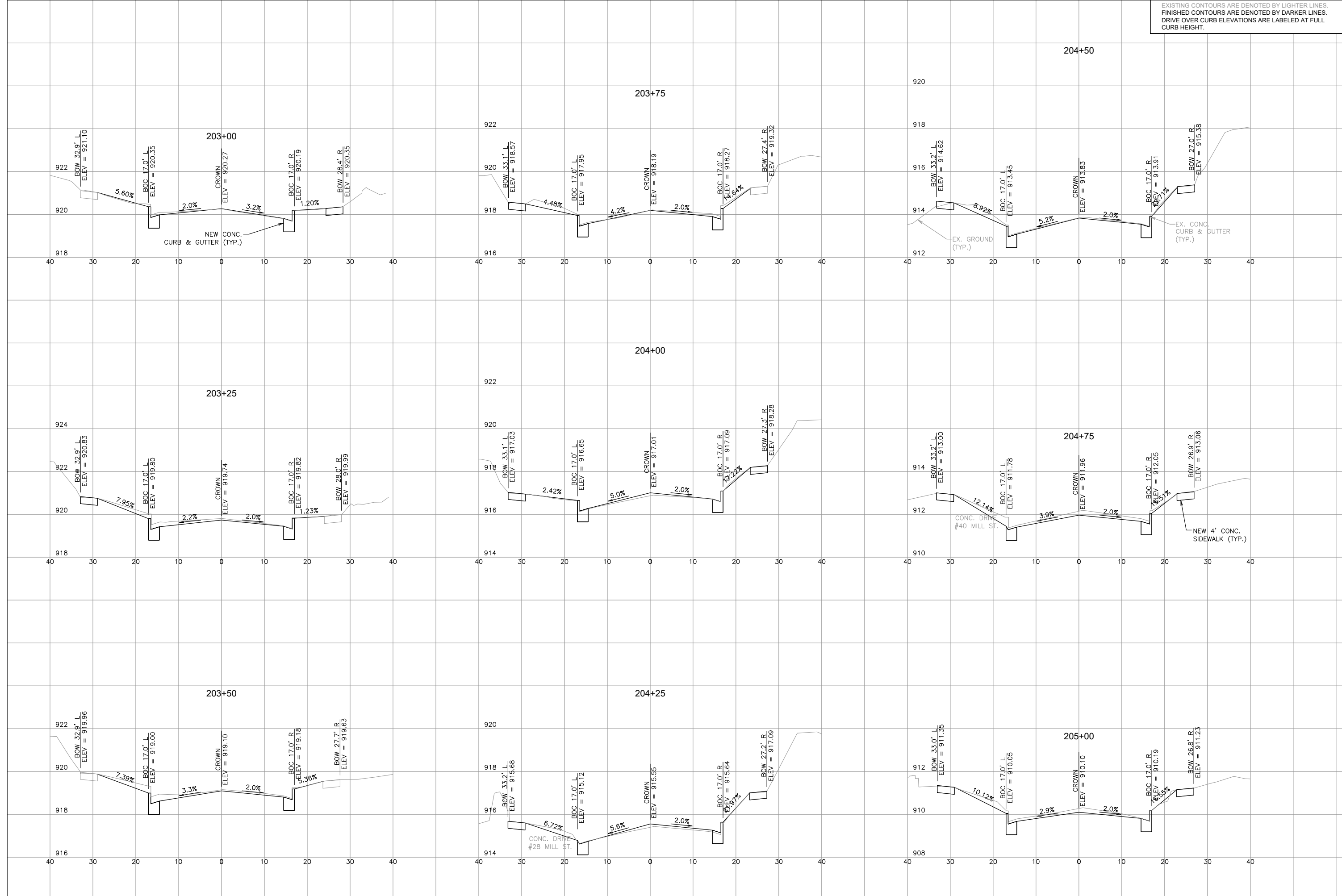


CROSS SECTIONS  
 MILL STREET  
 Station 203+00 To Station 205+00

2025 STREET AND UTILITY IMPROVEMENTS  
 Cherry St., Mill St., and Railroad St.  
 City of Evansville, Wisconsin

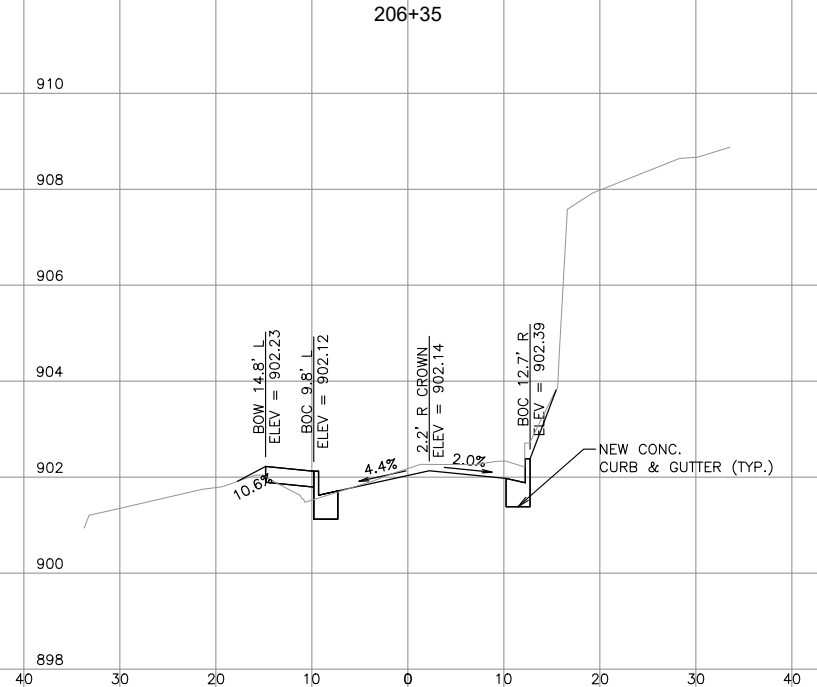
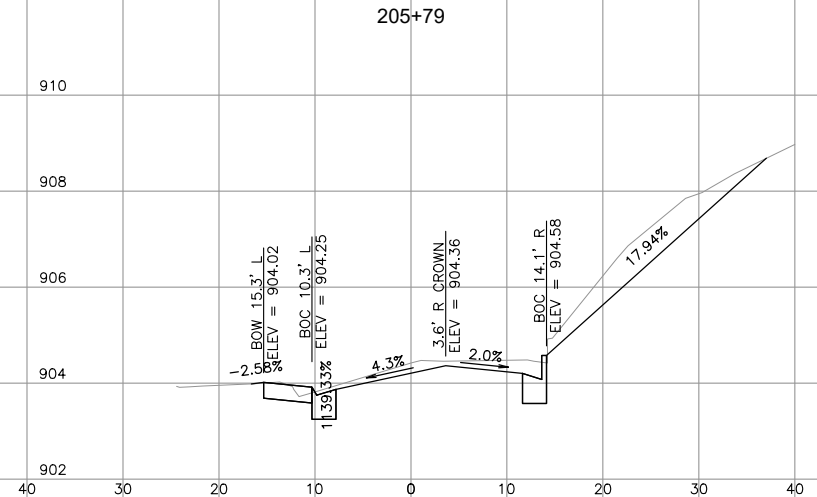
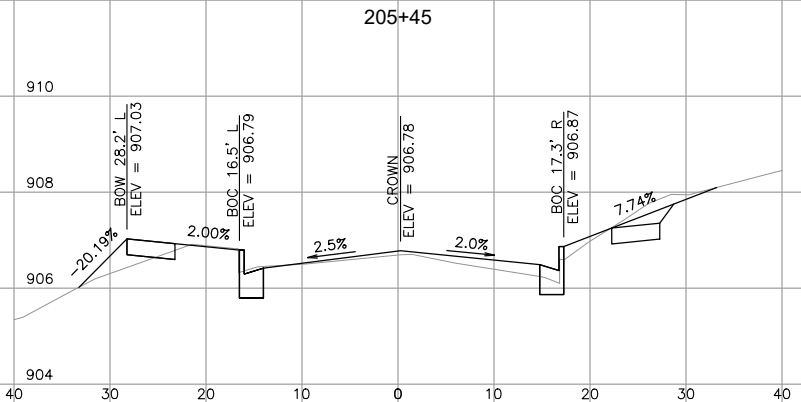
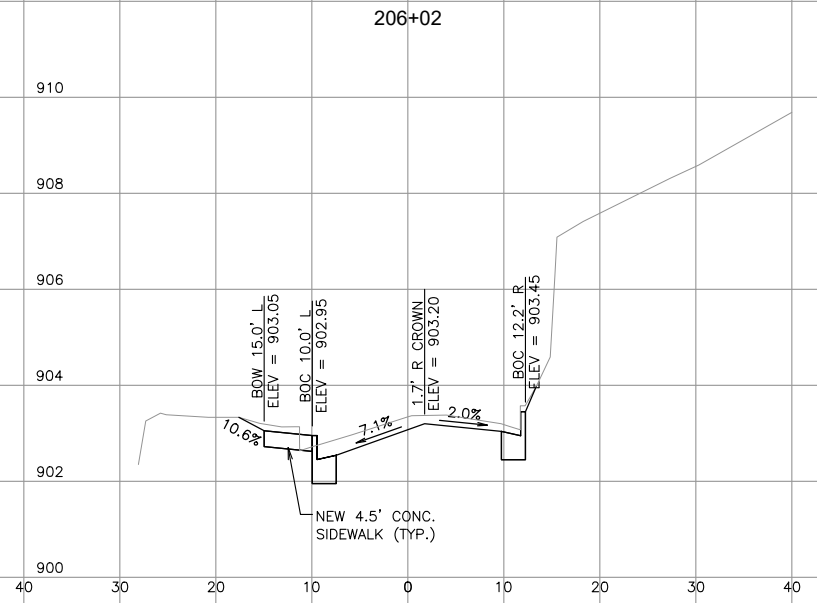
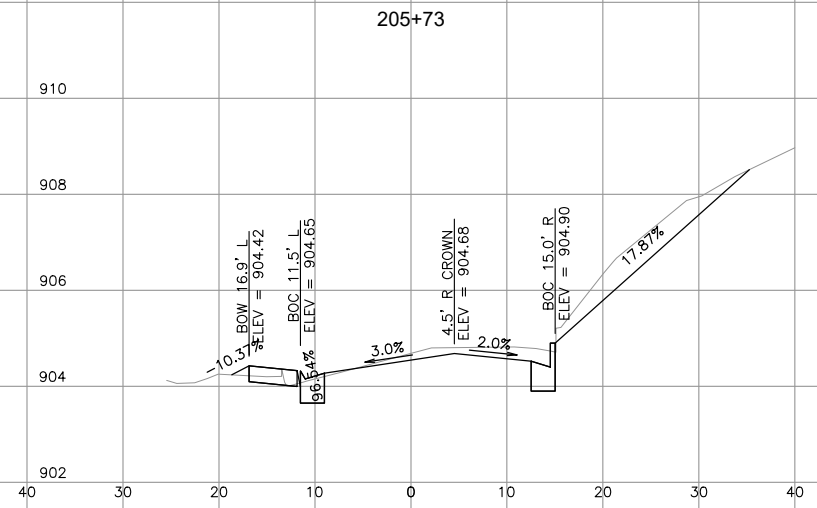
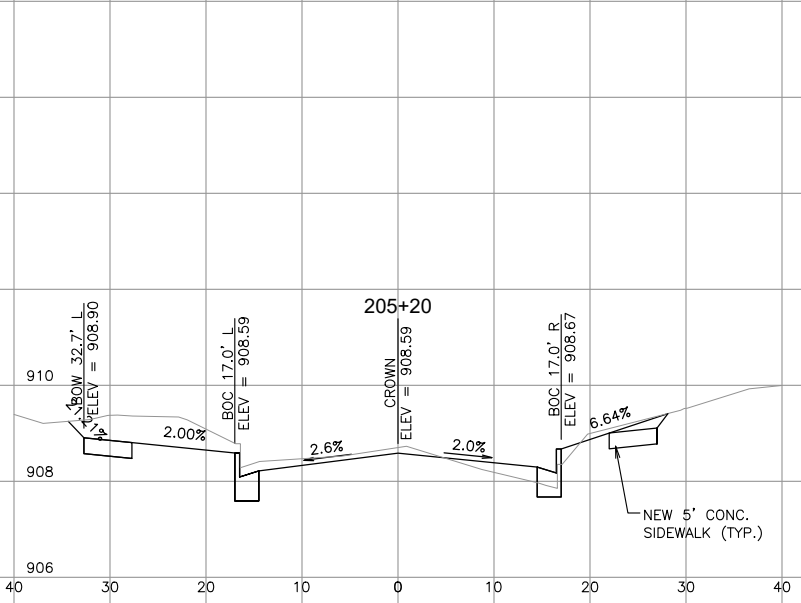
PROJECT NO.:  
 EV 124  
 DRAWING FILE:  
 EV124 CORRIDOR.DWG  
 DRAWN BY:  
 A.M.C.  
 CHECKED BY:  
 N.R.B.  
 DATE:  
 2-14-25  
 REVISIONS:  
 SCALE: HORIZONTAL  
 0 2.5 5 10  
 VERTICAL  
 1 2  
 SHEET:

X10





EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



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CROSS SECTIONS  
 RAILROAD STREET  
 Station 205+20 To Station 206+35

2025 STREET AND UTILITY IMPROVEMENTS  
 Cherry St., Mill St., and Railroad St.  
 City of Evansville, Wisconsin

PROJECT NO.:  
 EV 124  
 DRAWING FILE:  
 EV124 CORRIDOR.DWG  
 DRAWN BY:  
 A.M.C.  
 CHECKED BY:  
 N.R.B.

DATE:  
 2-14-25

REVISIONS:  
 SCALE: HORIZONTAL  
 0 2.5 5 10  
 VERTICAL  
 1  
 2

SHEET:  
 X11

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
CURB HEIGHT.

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Madison, WI 53719  
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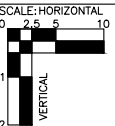


CROSS SECTIONS  
RAILROAD STREET  
Station 206+50 To Station 207+75

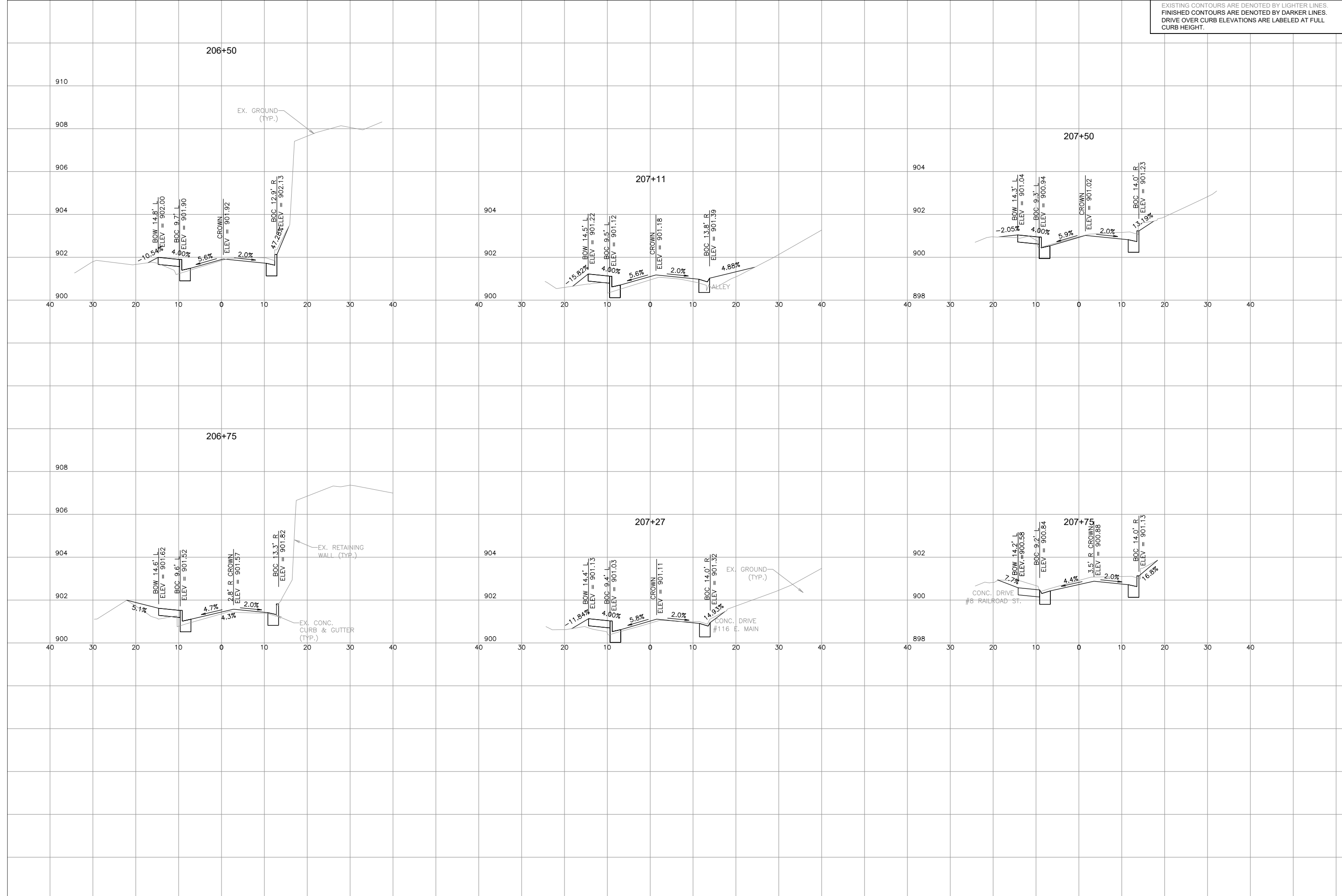
2025 STREET AND UTILITY IMPROVEMENTS  
Cherry St., Mill St., and Railroad St.  
City of Evansville, Wisconsin

PROJECT NO.:  
EV 124  
DRAWING FILE:  
EV124 CORRIDOR.DWG  
DRAWN BY:  
A.M.C.  
CHECKED BY:  
N.R.B.

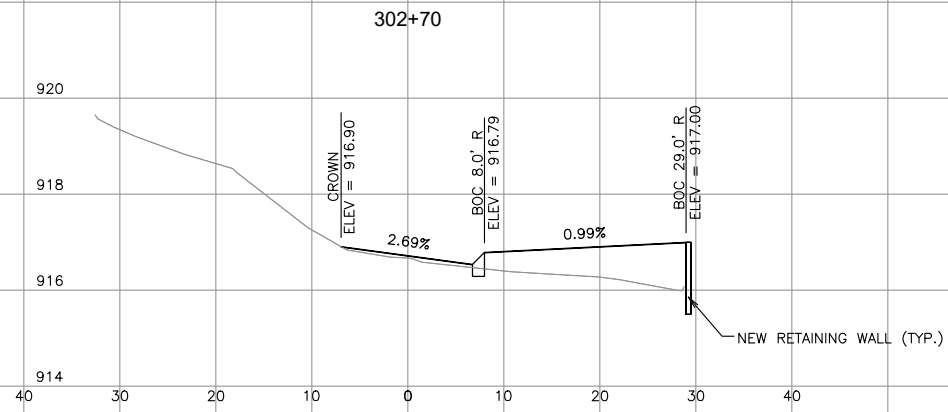
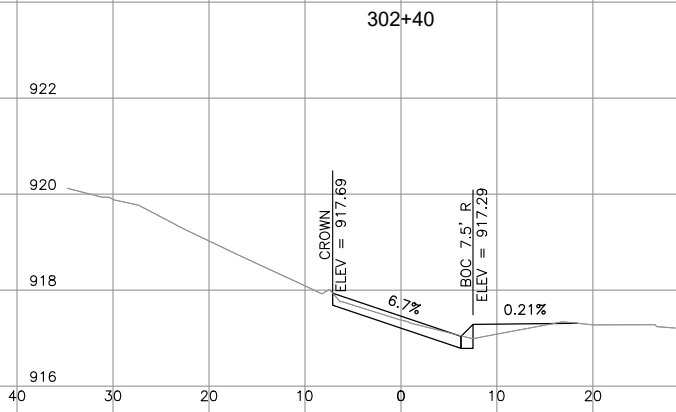
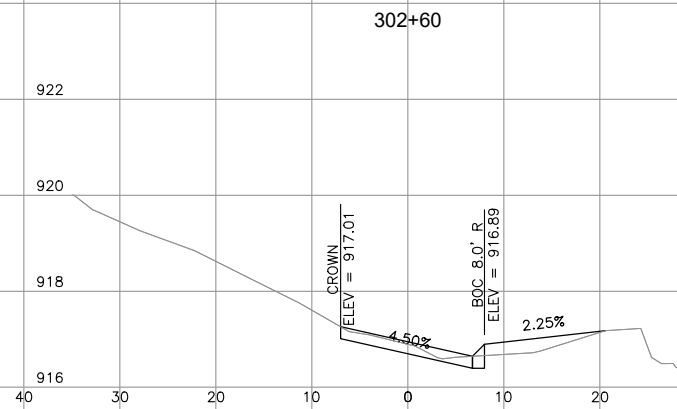
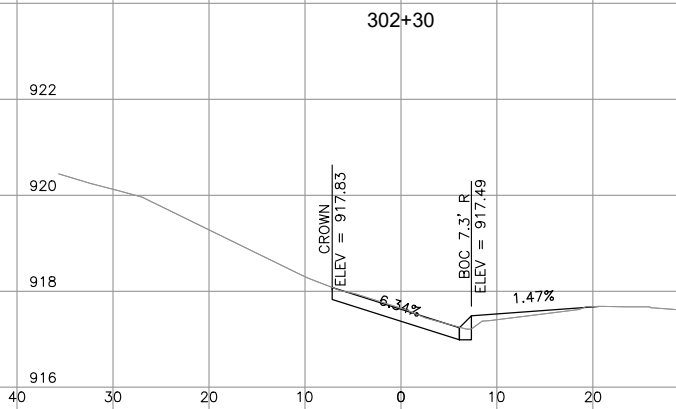
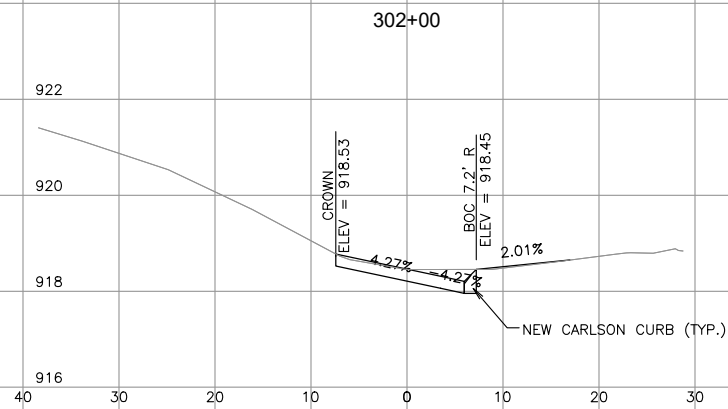
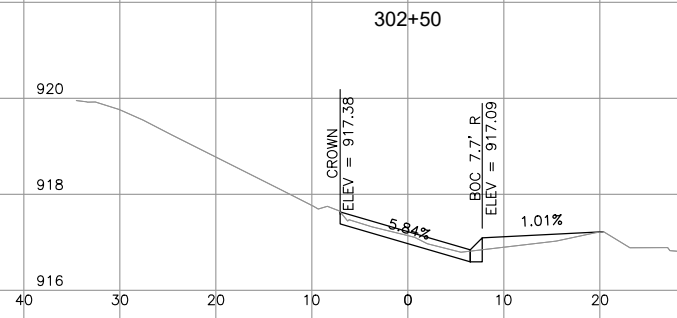
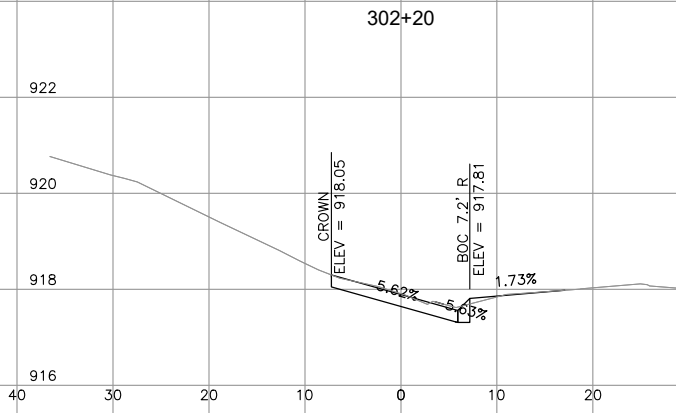
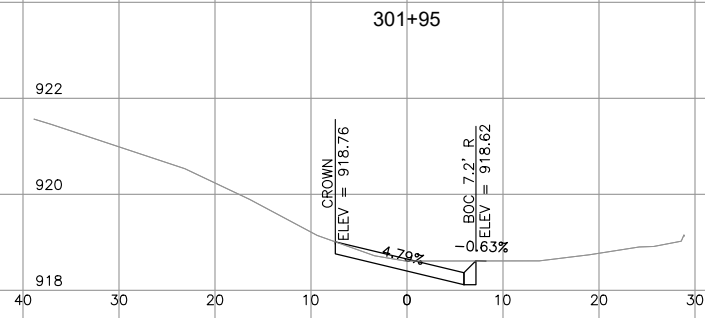
DATE:  
2-14-25  
REVISIONS:



SHEET:  
X12



EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



PROJECT NO.:  
EV 124

DRAWING FILE:  
EV124 CORRIDOR.DWG

DRAWN BY:  
A.M.C.

CHECKED BY:  
N.R.B.

DATE:  
2-14-25

REVISIONS:

SCALE: HORIZONTAL  
0 2.5 5 10

VERTICAL  
1  
2

SHEET:

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.

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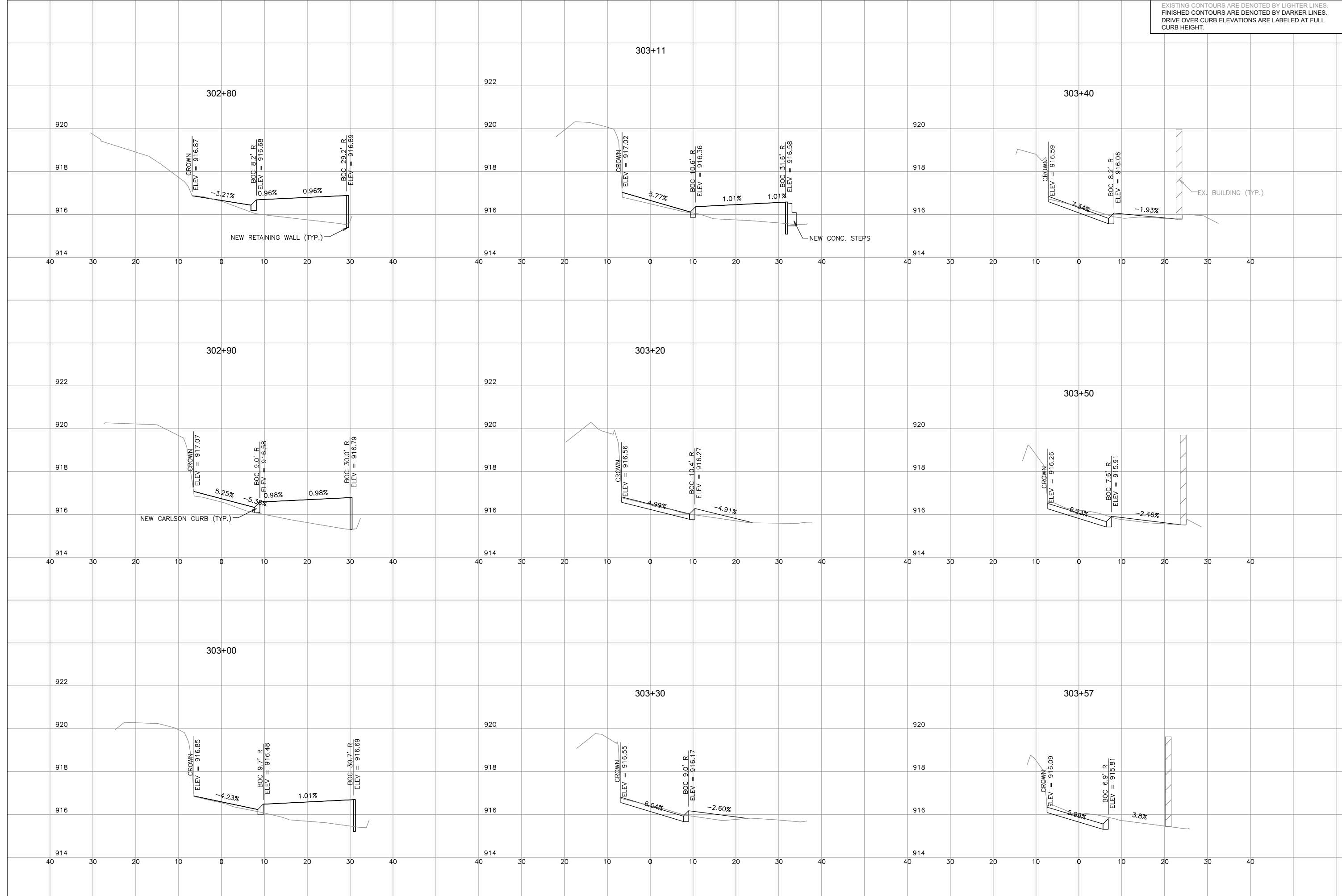
CROSS SECTIONS  
 RAILROAD ALLEY  
 Station 302+80 To Station 303+57

2025 STREET AND UTILITY IMPROVEMENTS  
 Cherry St., Mill St., and Railroad St.  
 City of Evansville, Wisconsin

PROJECT NO.:  
 EV 124  
 DRAWING FILE:  
 EV124 CORRIDOR.DWG  
 DRAWN BY:  
 A.M.C.  
 CHECKED BY:  
 N.R.B.  
 DATE:  
 2-14-25  
 REVISIONS:

SCALE: HORIZONTAL  
 0 2.5 5 10  
 VERTICAL  
 1 2  
 SHEET:

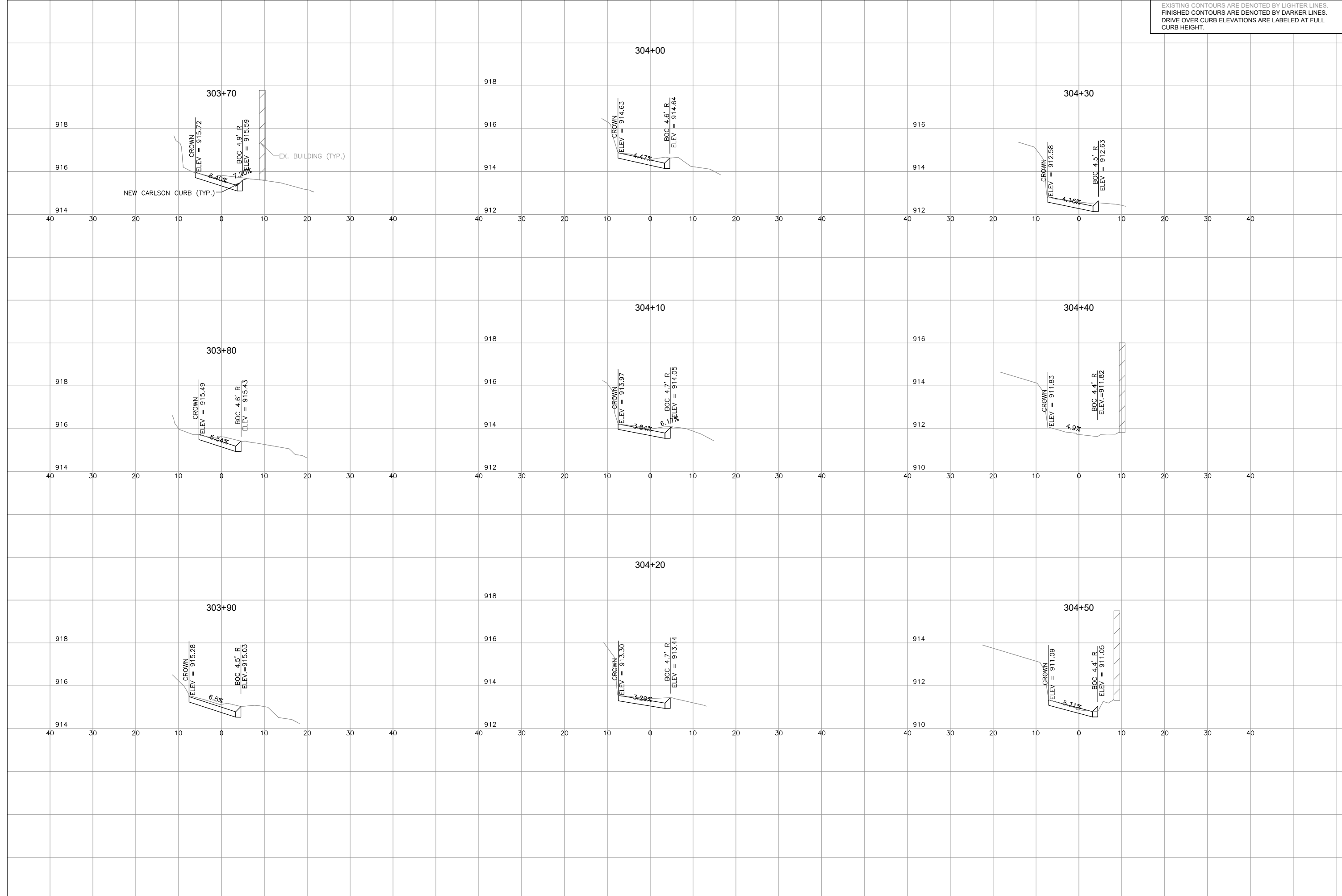
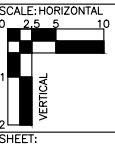
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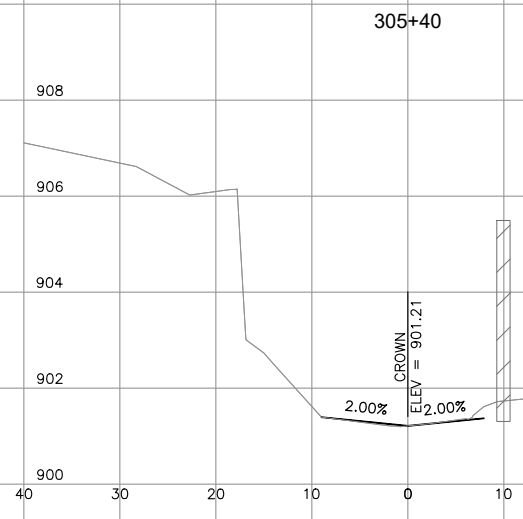
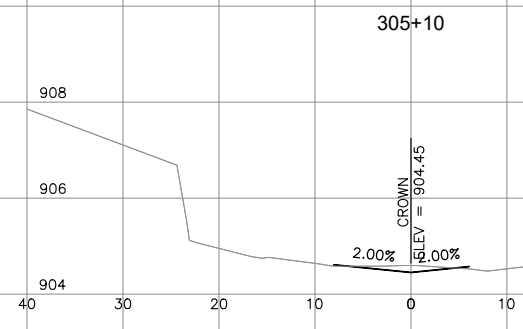
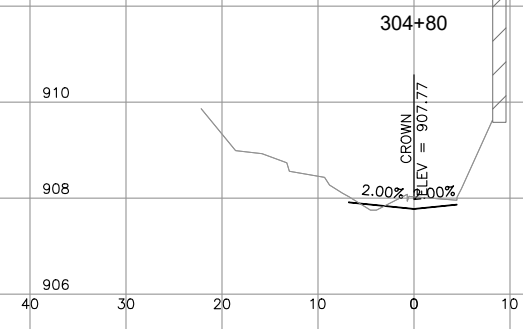
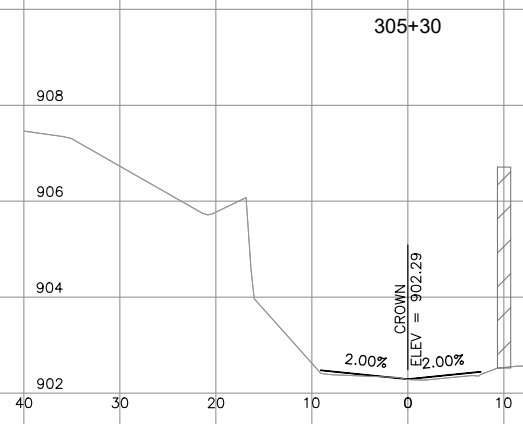
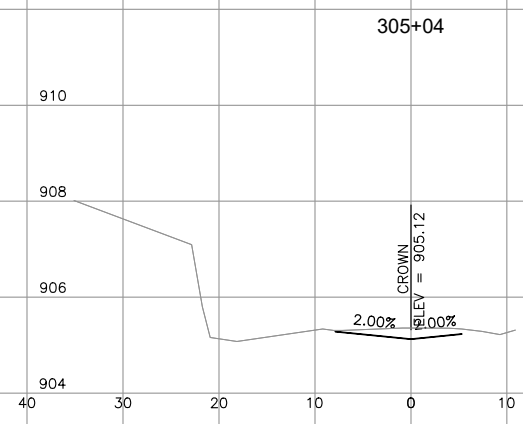
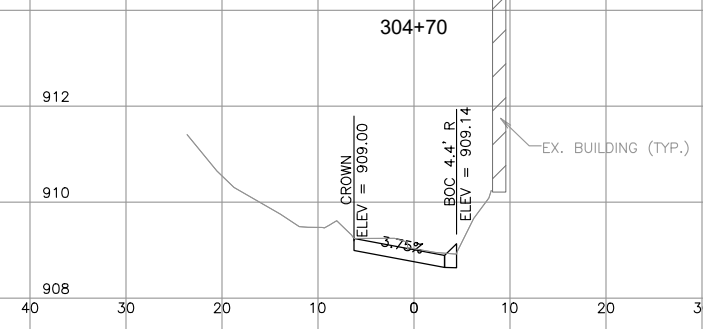
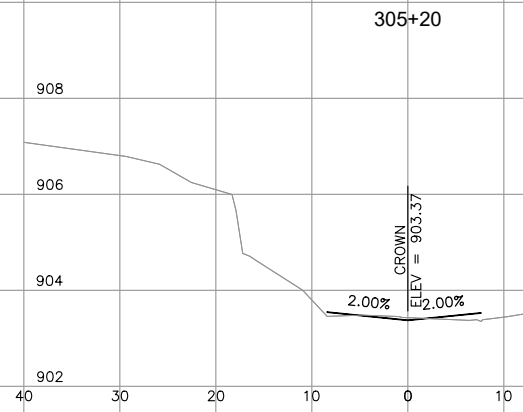
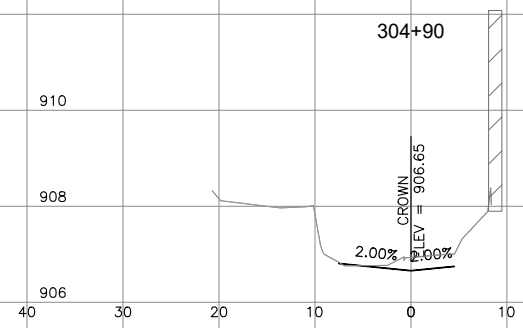
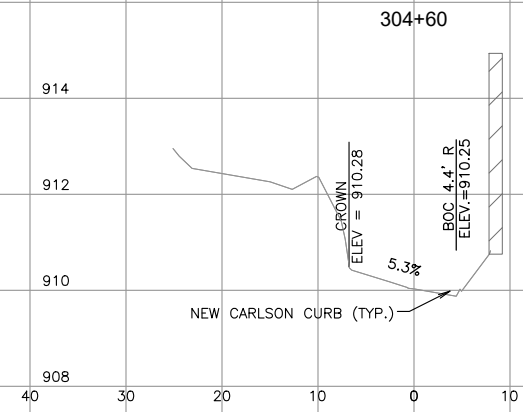
EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



PROJECT NO.:  
 EV 124  
 DRAWING FILE:  
 EV124\_CORRIDOR.DWG  
 DRAWN BY:  
 A.M.C.  
 CHECKED BY:  
 N.R.B.  
 DATE:  
 2-14-25  
 REVISIONS:

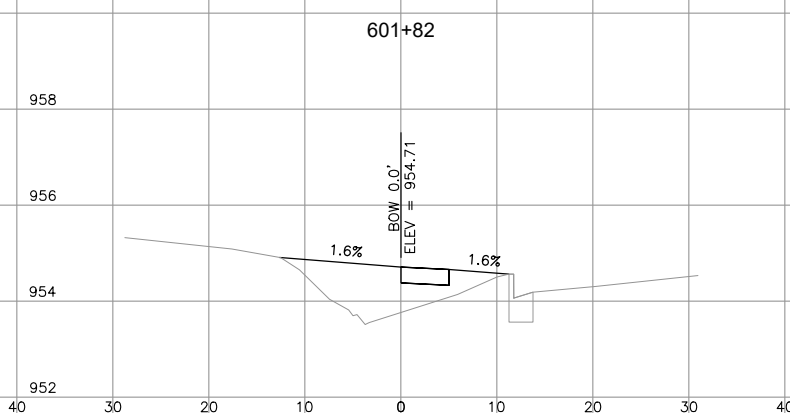
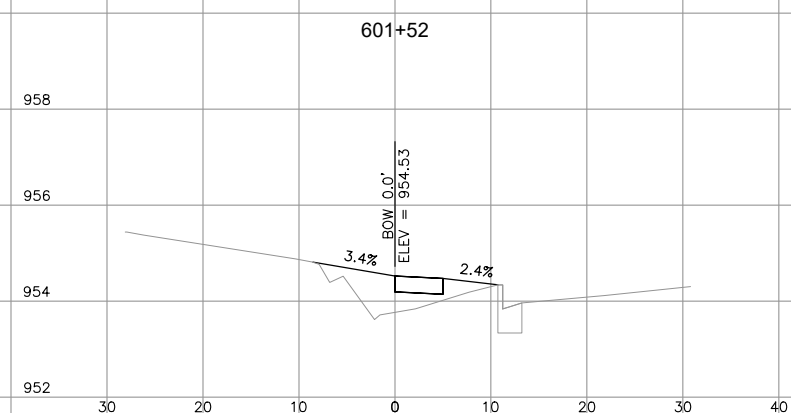
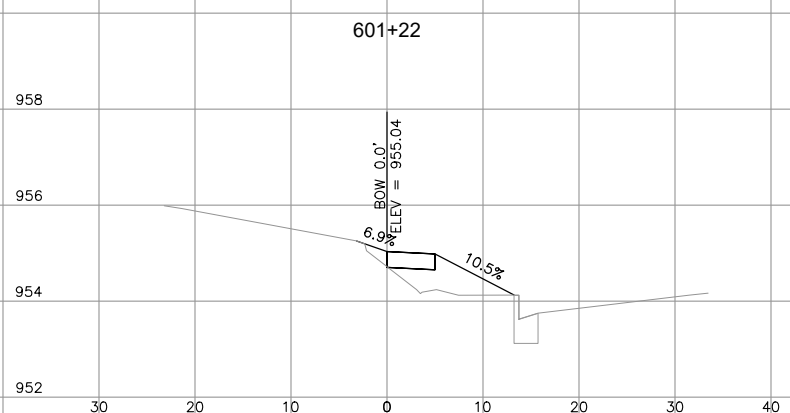
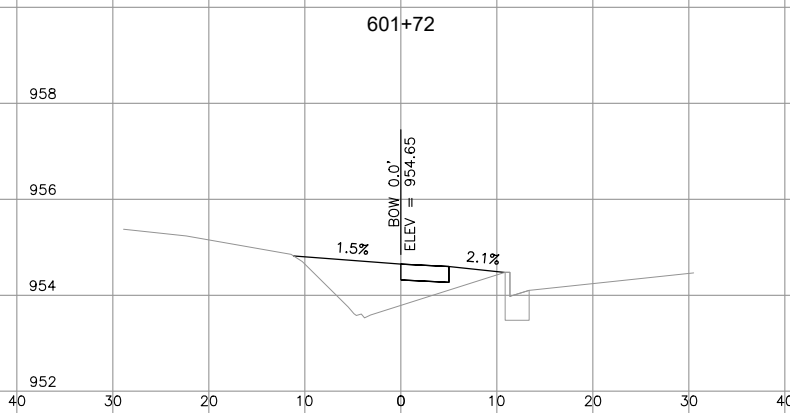
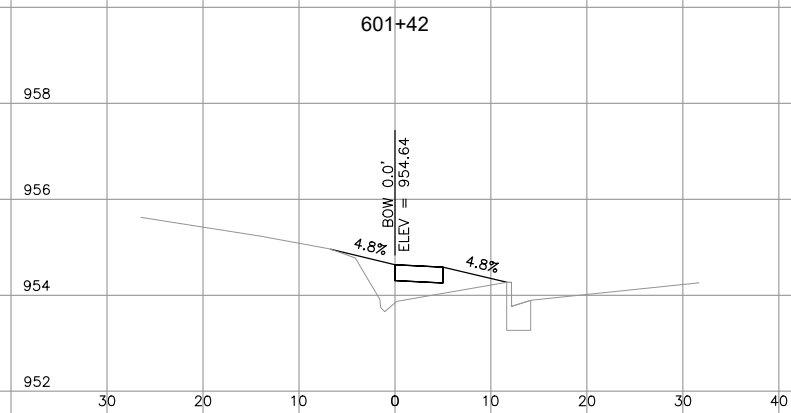
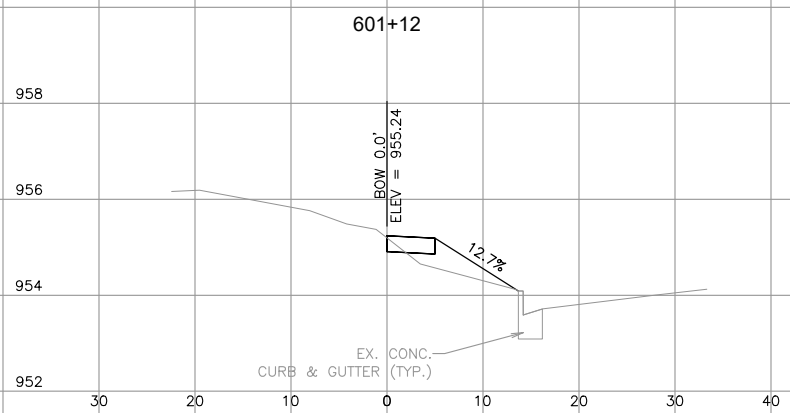
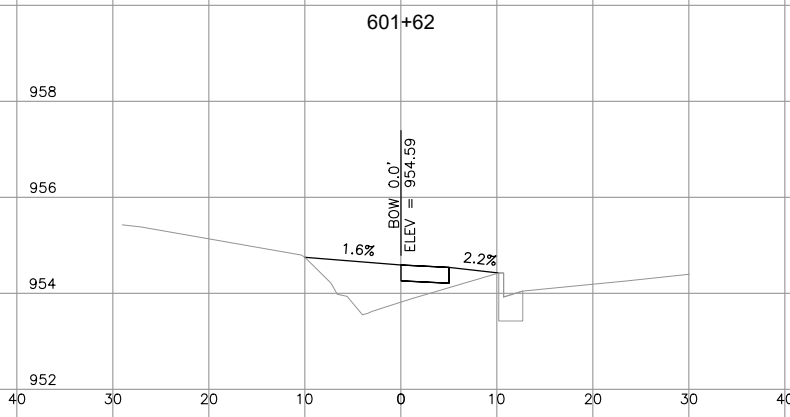
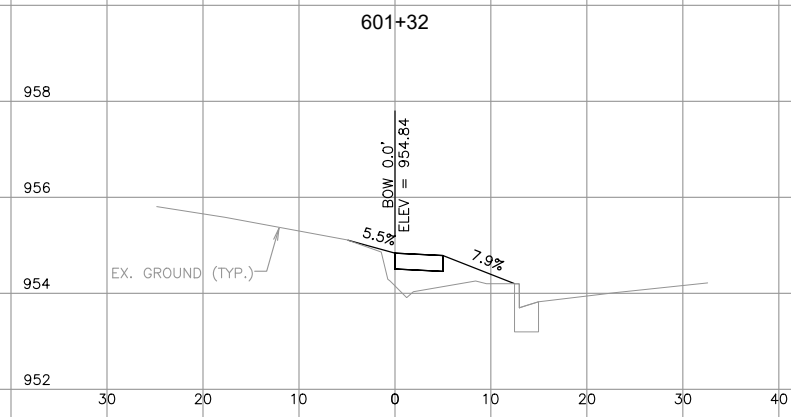
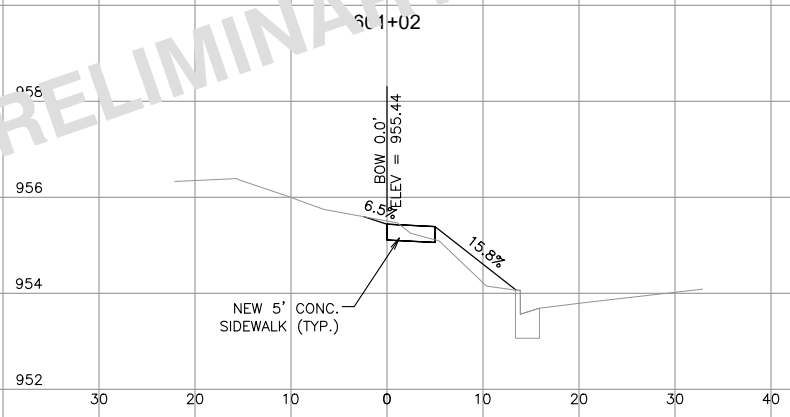


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 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



PRELIMINARY

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
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CURB HEIGHT.



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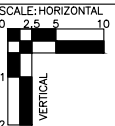


CROSS SECTIONS  
GARFIELD AVENUE  
Station 601+02 To Station 601+82

2024 STREET AND UTILITY IMPROVEMENTS  
Cherry Street, Mill Street, and Railroad Street  
City of Evansville, Wisconsin

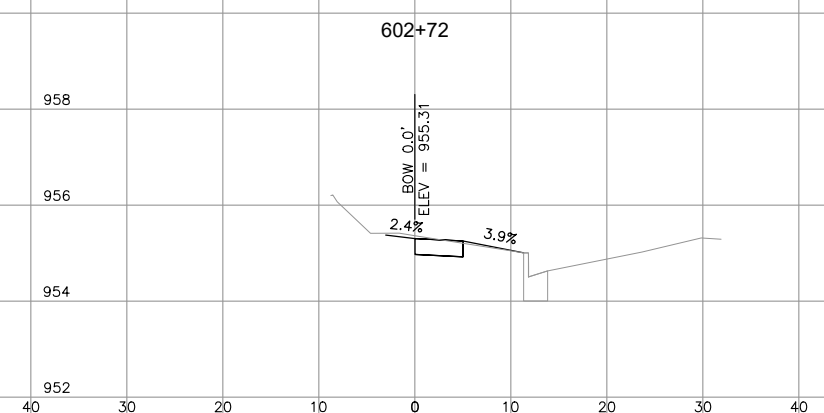
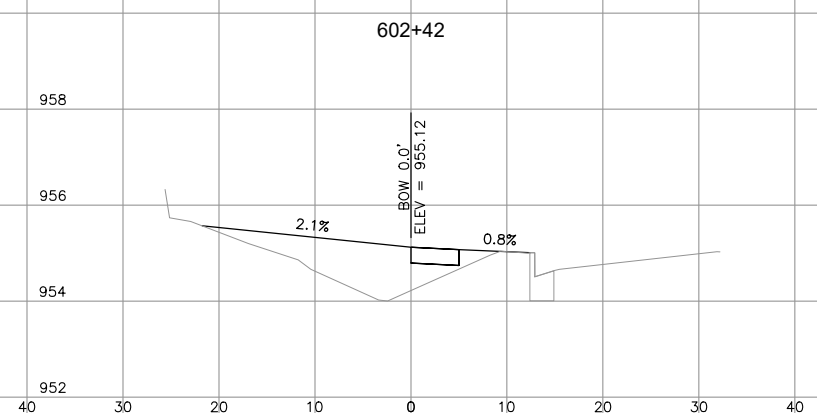
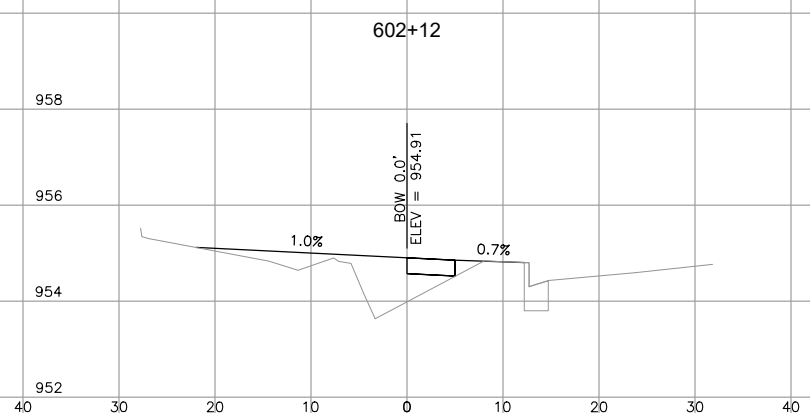
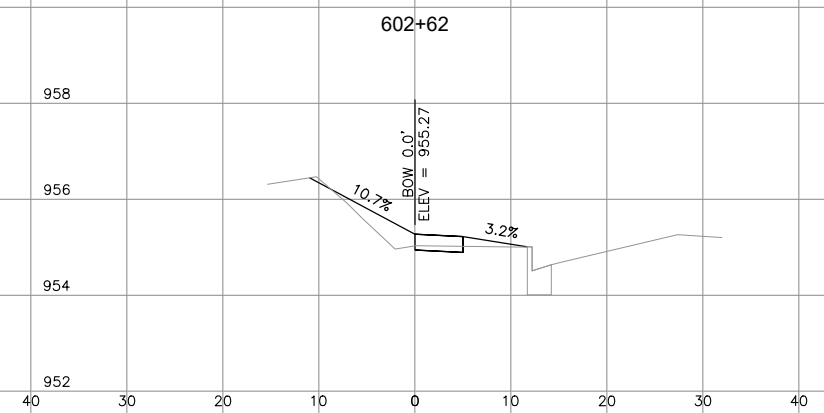
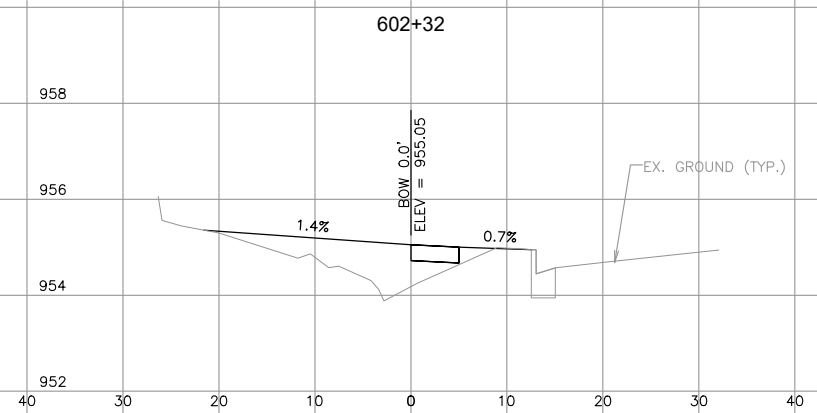
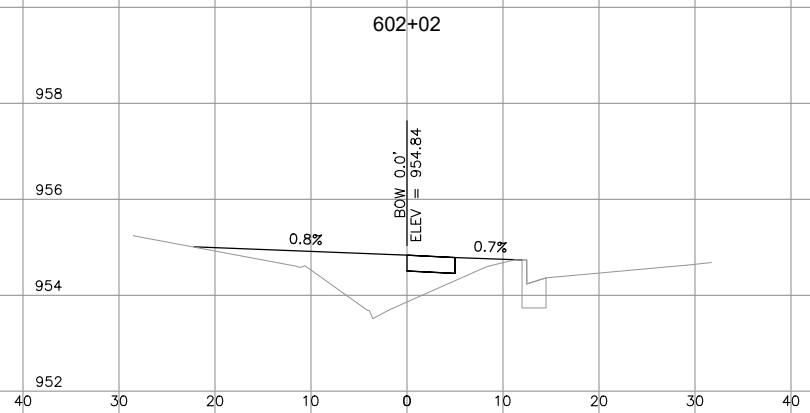
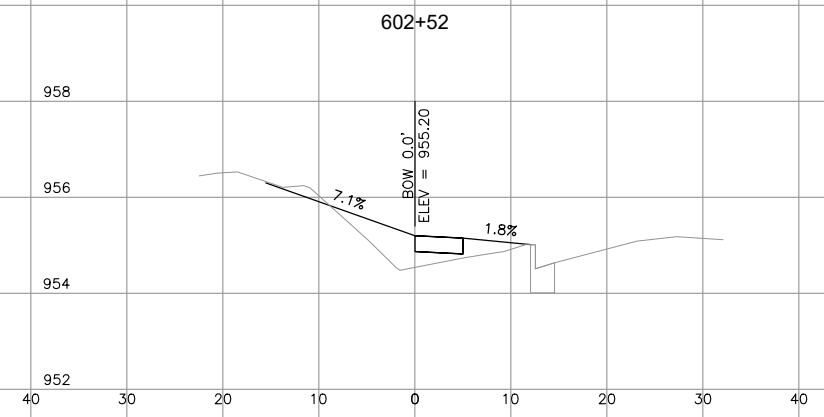
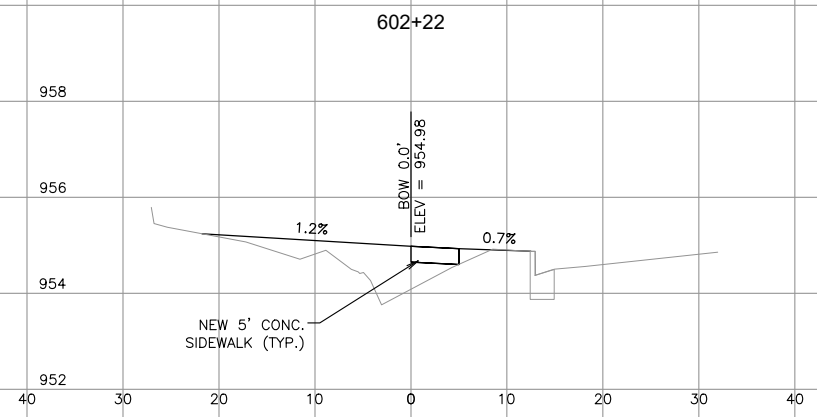
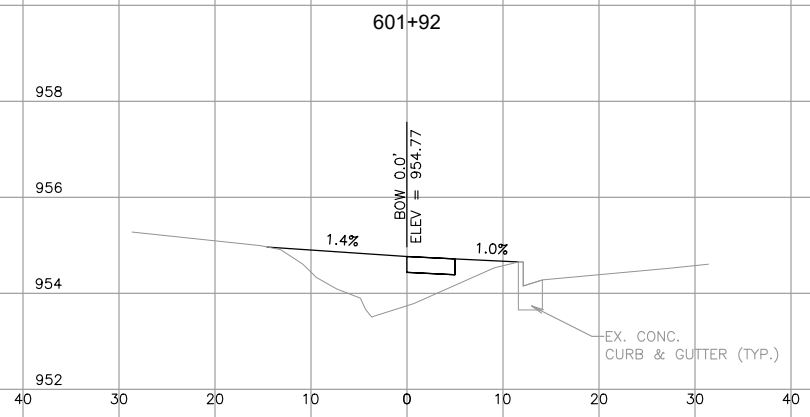
PROJECT NO.:  
EV 124  
DRAWING FILE:  
EV128 BASE.DWG  
DRAWN BY:  
A.M.C.  
CHECKED BY:  
N.R.B.

DATE:  
2-14-25  
REVISIONS:



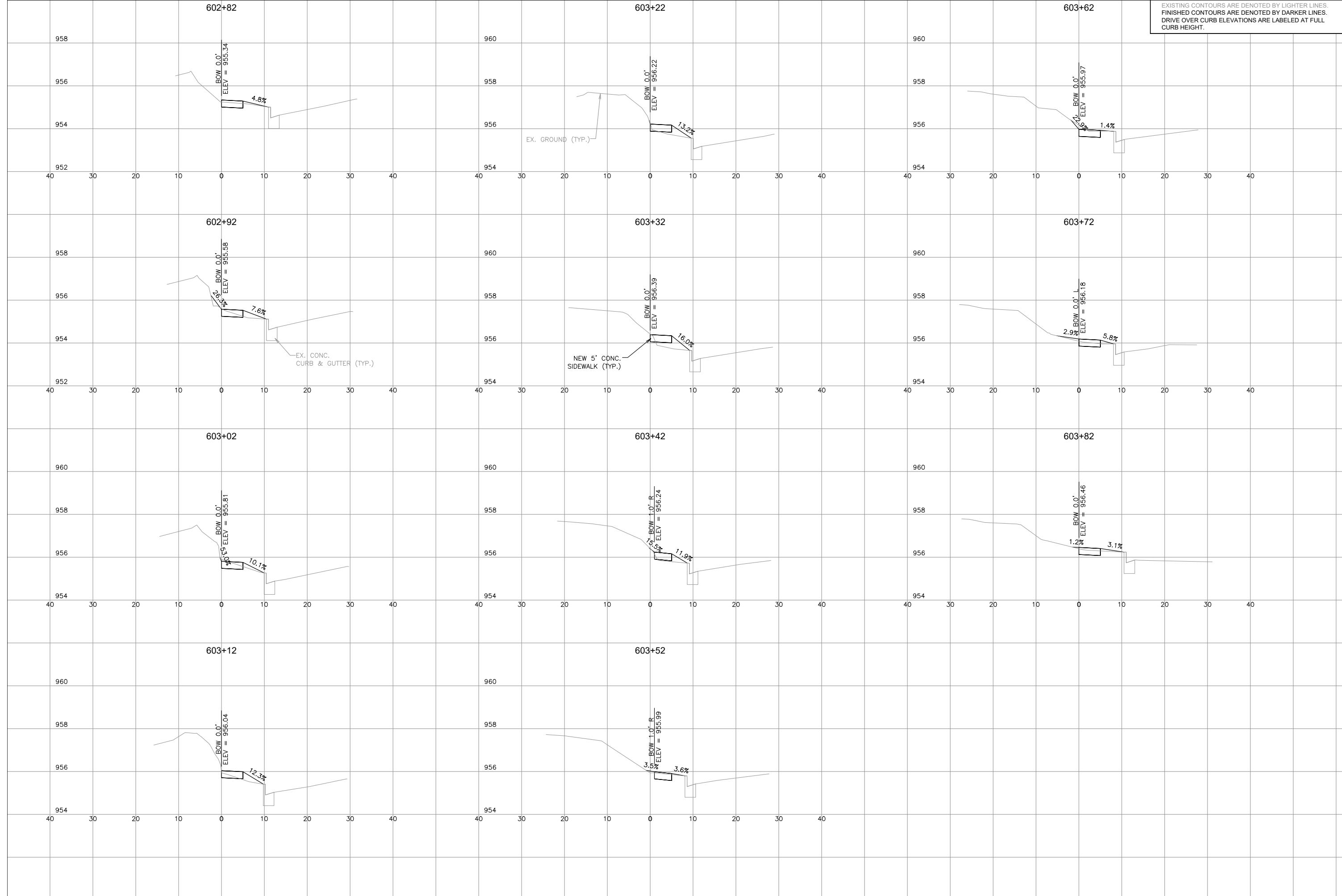
SHEET:  
X17

EXISTING CONTOURS ARE DENOTED BY LIGHTER LINES.  
 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.





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 FINISHED CONTOURS ARE DENOTED BY DARKER LINES.  
 DRIVE OVER CURB ELEVATIONS ARE LABELED AT FULL  
 CURB HEIGHT.



Station 602+82 To Station 603+82